



**CITY OF CARLTON
PLANNING COMMISSION MEETING AGENDA
MONDAY, MARCH 15, 2021, 6:00 P.M.
VIA ZOOM**

The Mission of the City of Carlton is to sustain and enhance the viability of the community by providing essential services with professionalism and integrity.

	<u>Pages</u>
1. Call to Order – Roll Call	
A) Changes to the Agenda	X
2. Approval of Minutes – February 22, 2021	2
3. Citizen Comments	
4. Public Hearing:	4
A) Site Design Review- City of Carlton; City File #SDR-2021-01 Vehicle building replacement at 191 East Main street	
5. Commissioner Comments	X
6. Adjournment	

Per the Governor’s Executive Order 20-16 on April 16, 2020, no public hearing comments will be taken in person. The building, City Hall, is closed to the public. This meeting will be held via Video Teleconference to meet public gathering requirements and to adhere to public health and safety standards. To attend or participate the meeting virtually, you can log in with a computer using the link below:

<https://us02web.zoom.us/j/89094732326?pwd=T3dkR3MwU0hSZGV0QllsUGNpM2RTZz09>

This meeting ID: 890 9473 2326

Passcode: 320743

Or you can call **1-253-215-8782**, input the meeting ID and password and enter the meeting using your phone.

Any public comments for topics on this Agenda item should be emailed to aamerson@ci.carlton.or.us by Thursday, March 11th in order to be shared with the Commissioners prior to the meeting. Comments can also be given verbally to Commissioners during the meeting. Thank you.

This facility is ADA accessible. If you need special accommodations, please contact Aimee Amerson at 503.852.7575 or aamerson@ci.carlton.or.us at least 24 hours prior to this meeting.



Planning Commission Regular Session Minutes
February 22, 2021, 6:00 PM
Via Zoom

1. CALL MEETING TO ORDER & ROLL CALL

Aimee Amerson called the meeting to order at 6:00 PM.

Members Present: Bob Graham Jennifer Nordstrom
 Anthony Stuart Jim Bandy
 Susan Turrell Randy Robbins
 Annette Fernandez Madrid

Members Absent: None.

Staff Present: Walt Gowell, City Attorney
 Aimee Amerson, Planning/Administrative Manager
 Kiel Jenkins, Interim City Planner
 Nicholas Peasley, Associate Attorney

Others: Marie Frugia, Linda Watkins and Grant Erickson

2. MINUTES APPROVAL- January 25, 2021

MOTION: Madrid/Stuart: to approve the Planning Commission minutes from January 25, 2021, as submitted. Motion carried (7 Yes/0 No/0 Absent/0 Abstain).

3. CITIZEN COMMENTS

6:03 PM

None given.

4. ACTION DISCUSSION ITEMS

A) 5G Design Review and Council recommendations

6:04 PM

Chair Graham introduced City Attorney Walt Gowell. Gowell read key points in the League of Oregon Cities Ordinance template that targeted some of the past Commissioner concerns and questions.

Commissioners asked questions of Gowell and Jenkins regarding waiting on a decision, costs involved, deviation options, historic district verses Carlton Downtown zone, fees to charge, locating facilities on historic poles and creating areas to test or try out 5G facilities.

Commissioners request to have a representative from the League of Oregon Cities and the City of Sherwood to offer answers and guidance to the Commission and City Council in a joint work session meeting.

MOTION: Bandy/Fernandez-Madrid: move to invite a representative from the League of Oregon Cities and the City of Sherwood Planning Department to speak, advise and guide future discussions on two different meeting dates with both City Council and Planning Commission in a joint meeting. Motion carried (7 Yes/0 No/0 Absent/0 Abstain).

5. COMMISSIONER COMMENTS

None given.

7:16 PM

6. ADJOURNMENT

The meeting adjourned at 7:19 PM.

ATTEST:

Aimee Amerson,
Administrative Manager

Bob Graham, Planning Commissioner Chair

NOTICE OF A PUBLIC HEARING
CITY OF CARLTON

NOTICE IS HEREBY GIVEN that the City of Carlton Planning Commission will hold a public hearing on **Monday, March 15, 2021 at 6:00 pm** via Zoom, to consider:

City File #Major SDR 2021-01; City of Carlton vehicle building replacement – Request for approval of a Major Site Design Review for the replacement of the vehicle building and addition of a parking lot.

The property is zoned Downtown District Railroad Sub-District (DD-RR) zone and is located at 191 East Main Street, further described as Assessors Map R3421AD, Tax Lot 02800.

The relevant standards and criteria are found in the Carlton Development Code as follows: Sections 17.30 Downtown District; 17.30.050 & 060 Dimensional & Development & Design Standards; 17.30.070 D-WG District Design Standards; 17.68 Off-Street Parking; 17.72 Storm Drainage & Utilities; 17.84 Site & Landscaping Design; 17.156 Site Design Review Process & Application; 17.192 Type II Actions and Procedures, and 17.196 Public Hearings.

HOW TO PARTICIPATE: Any person desiring to speak either for or against the proposal may do so in person or by authorized representative at the public hearing. In addition, written comments may be submitted prior to the hearing with the City Recorder at City Hall. The documents, evidence or staff report relied upon will be available for inspection at City Hall seven days prior to the hearing at no cost and will be provided at reasonable cost. Public comments shall address the relevant criteria. Failure of an issue to be raised in the hearing, in person or in writing, or failure to provide sufficient specificity to afford the Planning Commission an opportunity to respond to an issue means that an appeal on that issue cannot be filed with the State Land Use Board of Appeals.

The meeting is accessible to the disabled. If you have the need for special accommodation to attend or participate in the hearing, notify Aimee Amerson 24 hours before the hearing.

For further information or to review the file, please contact the Kiel Jenkins, City Planner at kjenkins@mwvcog.or or Planning Clerk, Aimee Amerson, at City Hall at aamerson@ci.carlton.or.us or (503) 852-7575.

PLEASE BE ADVISED- THIS MEETING WILL ONLY BE AVAILABLE VIA TELECONFERENCE. PLEASE SEE BELOW FOR MEETING ACCESS DETAILS.

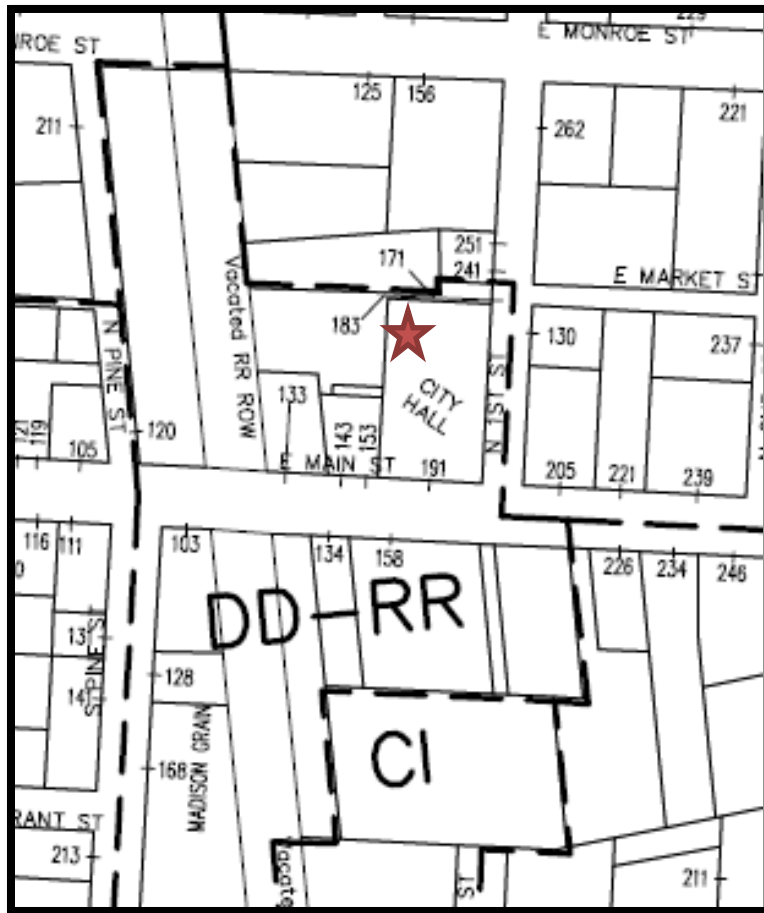
To join meeting with a computer, please follow this link:

<https://us02web.zoom.us/j/89094732326?pwd=T3dkR3MwU0hSZGV0QllsUGNpM2RTZz09>

To join using a phone, please call: 1-253-215-8782

Enter Meeting ID#: 890 9473 2326

Meeting passcode: 320743



**CITY OF CARLTON
PLANNING COMMISSION**

STAFF REPORT: Major Site Design Review

File# SDR 2021-01

HEARING DATE: March 15th 2021

FILE NUMBER: SDR 2021-01

REQUEST: Approval of a Major Site Design Review for the replacement of a vehicle storage building for the City of Carlton Police Department and a renovation of the adjoining parking lot.

LOCATION: 191 East Main St.
R3421AD 02800

ZONING: D-RR (Downtown- Railroad)

APPLICANT: City of Carlton

OWNER: City of Carlton

CRITERIA: Carlton Development Code Section 17.156

ENCLOSURES: Exhibit A1: Applicant's Materials- Response to Criteria
Exhibit A2: Applicant's Materials- Site Plan
Exhibit A3: Applicant's Materials- Building Plans
Exhibit A4: Applicant's Materials- Application form
Exhibit B: Engineer's Comments
Exhibit C: Vicinity Map

ACRONYMS:

ADA Americans with Disabilities Act	ORS Oregon Revised Statutes
SF Square Feet	PWDS Public Works Design Standards
PWDS Public Works Design Standards	SDR Site Design Review
R-1 Residential-Low Density	
PUE Public Utility Easement	
DD-WG Downtown District-Wine Gallery	
ROW Right-of-way	
CDC Carlton Development Code	
CMC Carlton Municipal Code	
CUP Conditional Use Permit	
DEQ Department of Environmental Quality	
IG General Industrial Zone District	
OAR Oregon Administrative Rules	
ODOT Oregon Department of Transportation	
OFC Oregon Fire Code	
OPC Oregon Plumbing Code	
ORD Ordinance	

I. REQUEST

This is a two-part Site Design Review with the following components:

1. The replacement of an existing building used for storage of police vehicles.
2. Renovations to the existing parking lot including additional parking, landscaping, and sidewalk improvements.

The applicant has indicated that this is the first step in a series of improvements to the City property.

II. AUTHORITY & PROCEDURE

According to CDC 17.144.030.C Major Site Design Review are processed as a Type II procedure. Type II procedures are conducted as stated in CDC 17.188.020. The Planning Commission is the final arbiter of a Type II application.

The application was received by the City on February 11th, 2021 and deemed complete on February 18th, 2021. The City has until June 18th, 2021, or 120 days from acceptance of the application to approve, modify and approve, or deny this proposal.

CDC 17.156.040 states that a major site design review shall be applicable to all new developments and major expansion or remodel (twenty-five (25) percent or more increase in total square footage) of existing developments except single-family detached dwellings and individual duplex dwellings (not part of a subdivision proposal) do not require site design review. Major site design review also applies to a development or change in use that requires a new access permit for access onto a state highway, requires the development of additional off-street parking, or increases AM or PM peak hour automobile trips to/from the site by more than ten (10) percent or ten (10) vehicles, whichever is greater. For purposes of this criterion, the current use as of the time of site design review application submittal shall provide the basis for calculating vehicle trips. Where the subject development is not currently in use, the most intensive use of the site during the past twelve (12) months shall serve as the basis for calculating proposed changes in parking and traffic.

CDC 17.156.070 states that as part of a site design review approval, the decision body may impose the following conditions on a new or expanding development to ensure compliance with the city's public facility standards:

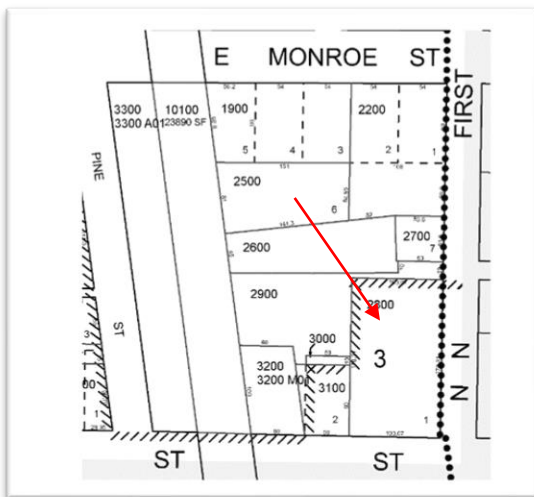
- A. Limit or prohibit access to streets, including requiring consolidation or reconfiguration of existing accesses;
- B. Require transportation improvements, including but not limited to new or widened streets, sidewalks, bicycle lanes, on-street parking, roadway markings, traffic controls, bus transit waiting areas, or other improvements consistent with the city's transportation system plan or as recommended in a traffic impact analysis pursuant to Chapter 17.100
- C. Require the dedication of additional right-of-way and/or street improvements where necessary to meet city street standards.
- D. Require the replacement, extension, and/or upgrade of other essential infrastructure, public or private, including but not limited to water, sewer and storm drainage facilities, provided the required improvements must be necessary to serve the proposed development in conformance

with city standards, and the cost borne by the developer/applicant must be roughly proportional to the impact the development is expected to have on those facilities.

III. SITE AND NEIGHBORHOOD ANALYSIS

Existing Zoning and Development

The subject property is currently zoned D-RR (Downtown district, Railroad subdistrict) and is approximately .42 acres (19372 SF) in size. There are three buildings located on the subject property: City Hall and two storage buildings. One of the storage building is used for archiving of City documents and is not affected by this application. The proposed building is a replacement for second storage building, currently used as a storage building for the Police Department.



Surrounding properties to the North and West are also zoned D-RR and are occupied by the Police Department Building and a dentist's office. Property to the South is across Main Street and zoned D-RR. Property to the East is across 1st Street (Residential Medium-High Density).

IV. MAJOR SITE DESIGN REVIEW CRITERIA AND FINDINGS

The applicant is proposing to replace an existing storage building and make improvements to the site including parking lot and sidewalk improvements. The applicable review criteria for a Site Design Review may be found in CDC 17.156.060.

17.156.060(A). Conformance with the General Development Standards Sections 17.60 through 17.104.¹

17.60.020 Application of standards

B. Those development standards which are unique to a particular district shall be set forth in the section governing that district.

FINDING: The subject property is located within the Downtown District- Railroad Subdistrict. As such, the site is subject to the development and design standards listed in Chapter 17.30 as follows:

17.30.020- Permitted uses

K. Public and/or institutional uses, including public parking; and new on-premise parking subject to a public shared parking agreement.

FINDING: City Hall, public parking, and police department vehicle parking and storage are permitted uses within the D-RR zone.

17.30.050- Dimensional Standards

Minimum lot area- none

FINDING: There are no new lots or property line adjustments requested as part of the application. The subject lot is 18,372 square feet in size.

Minimum Front/Street Side Yards- D-RR: None, except setback area provided must be landscaped or serve as an extension of the sidewalk (e.g., outdoor café or plaza), except where access drives/alleys preclude landscaping.

FINDING: There are no required setbacks or yards. The proposal meets requirements.

Minimum Rear Yard- None, except 15 feet when abutting a residential district or city-owned plaza or parking lot.

Minimum Side Yard- None, except 10 feet required when abutting a residential district or city-owned plaza or parking lot, and as required for Street Side Yards.

Minimum and Maximum Structure Height- D-RR: 35 feet, except existing grain elevator is permitted and may be rebuilt in current location to original height.

FINDING: None of the subject property's boundaries abut residential zoning. The Northern property line (rear) is approximately 11 feet from a residential property, but is separated by an existing access drive. All other property lines abut either 1st street (side), Main St. (front) or property zoned DD-R (side). No setbacks are required. The building height of the proposed building is 14 feet, below the maximum structure height.

17.30.060- Development and design standards

D. Design Guidelines

1. Does the proposal contribute to the attributes that make the subject downtown sub-district (D-MS, D-WG, and/or D-RR) distinct? For example, does the proposal respond to the building forms, scale, setbacks, orientation, architectural style, materials, detailing, color, signage, parking, and/or other elements that distinguish the sub-district without mimicking other building designs? Is the proposal compatible with existing structures that have retained their historic integrity? It is not the City's intent to create an architectural theme, but rather to ensure that new buildings and alterations fit the historic context.

*Note: The applicant has requested several adjustments to the design standards found Chapter 17.30.060 due to the security needs of the proposed building as referenced in the findings below.

FINDING: The applicant has indicated that the existing structure to be replaced is currently in disrepair. The proposed building is of similar appearance and location and fits the architectural intent of the RR subdistrict.

2. If the proposal involves remodeling a building listed on a local state, or national historical register, is the remodel consistent with the guidelines for altering such historic resources?

FINDING: The proposed building is not on a historical record or register, therefore subsection 2 does not apply.

3. Does the proposal enhance the streetscape or other public spaces with appropriate building placement, orientation, height, architectural detailing and landscaping?

FINDING: As mentioned, the new building will replace the current building which is currently in disrepair. In addition, the proposal includes increased public parking, street and sidewalk upgrades, and landscaping thereby enhancing the aesthetics of the subject property.

4. If located at the intersection of Main Street/Pine Street, Main Street/Yamhill Street, or Pine Street/Grant Street, does the design enhance the "gateway" location with a corner plaza or vertical building elements (e.g., increased height) at the corner?

FINDING: The subject property is not located at any of the above locations. Subsection 4 is therefore not applicable.

5. Does the structure have a compatible building scale relationship with adjacent residences? For example, does the structure step-down in height adjacent to single family dwellings?

FINDING: Per the applicant and verified by staff, the new structure is in scale with surrounding residences and buildings. There is a two-story dental office, and a two-story single-family residence adjacent to the North. The proposed storage building is the same height as the other shop structure on the subject property located four feet to the South.

6. Does the building design address all four sides of the building with a unified design? For example, are the materials, textures and colors on each elevation coordinated?

FINDING: The applicant has indicated that the exterior walls and the roof of the building will be sheet metal. The color of the building is yet to be determined, but will be subject to staff review prior to approval of building permits. A Condition of Approval has been added requiring staff signoff of this standard to ensure the color meets the downtown color palette.

7. Where a zero-setback is proposed, does the side elevation facilitate common wall development in the future?

FINDING: A zero setback is existing and is proposed to be maintained along the West side of the building. The storage building does not facilitate common wall development. However, the property abutting the subject property to the West is already developed with a structure and parking, therefore making future common wall development unlikely.

8. Where the proposal includes an adjustment to the window transparency standards or other detailing standards, does the proposed design adequately break up the building elevation (avoid creating a blank wall) and express storefront character in other ways?

FINDING: There are no windows proposed for the storage building because the building is to be used as storage for police vehicles and equipment and therefore needs to be secure. The applicant has requested an **adjustment** from the Planning Commission due to the security needs of the building. Staff finds that the adjustment is warranted because the building is:

1. Not a storefront as such is not easily viewable from the sidewalk AND
2. Includes four roll up doors on the front of the building to break up the building elevation.

9. Does the building contain openings (doors or windows) adjacent public spaces or parking areas, including those that may also serve as plazas or community gathering places during special events?

FINDING: The building will have roll up doors, similar to the existing building to be replaced. The building will not be accessible to the public due to its proposed use. The building is accessory to the primary structure (City Hall).

10. Do the facade and roofline have a rhythm that is consistent with adjacent buildings, or appropriately transition from one building to another?

FINDING: As described above and show in Exhibit B, the proposed building is metal and has a pitched roofline. The building is similar to the existing storage building remaining on the property and currently used as an archive for City Records. As previously discussed, the color of the building will be reviewed by staff for consistency with the downtown color palette prior to approval.

11. Does the proposal contain adequate sidewalks? Sidewalks must contain a sufficient pedestrian through zone (clearance) and Americans with Disabilities Act accessibility. Sidewalks within the MS sub-district must also contain a furnishing zone, per Section 17.30.070.

FINDING: The subject property is not located in the MS zone, and there are no existing sidewalks along 1st street. Sidewalks will be placed on 1st street along the frontage of parking area as required and covered in Section 17.64 of the CDC. All sidewalks will be ADA compliant.

12. If located adjacent to a plazas or area with curb extension or widened sidewalk, does the proposal include benches, café seating, or public art, per Section 17.30.070?

FINDING: The subject property is not located adjacent to any of the above and no additional benches or outdoor seating are proposed at this time.

13. If the proposal adjusts the lot coverage standard or contains on-premise parking, does it manage storm water drainage more effectively than would be possible under a conventional design? Does it utilize on-site retention with water quality features?

FINDING: Findings related to parking, storm drainage, and retention may be found in the section of this report addressing CDC chapters 17.68 and 17.72 below. The lot coverage standard will not be adjusted.

14. Does the proposal promote water conservation, for example, through drought-tolerant plantings or capturing rainwater for use in landscape irrigation?

FINDING: The applicant has indicated that any flowerbeds proposed within the landscape strips will include drought tolerant plantings. There is not sufficient space on the site for rainwater collection. The trees to be planted on site will be irrigated as indicated in Exhibit A2- Site Plans.

15. If the proposal leaves a gap between buildings (non-common wall development), does it provide landscaping (e.g., courtyard garden), or a plaza, with seating in that area?

FINDING: Subsection 15 does not apply to this application.

E. Building Orientation

1. Comply with the dimensional standards in subsection 17.30.050.

FINDING: As shown in findings above (17.30), the proposed structure complies with applicable dimensional standards.

2. Provide at least one primary building entrance facing an abutting street (i.e., within 45 degrees of the street property line); or if the building entrance is turned more than 45 degrees from the street (i.e., front door is on a side elevation), the primary entrance shall open onto a pedestrian plaza or courtyard and a walkway shall connect the primary entrance to the plaza and sidewalk.

3. Development abutting Main Street shall orient to Main Street. Where a development does not abut Main Street, it shall provide a primary entrance facing the street that is likely to have the most pedestrian traffic, as determined by the Planning Commission.

4. Buildings on lots abutting a plaza or a parking/plaza area shall have at least one secondary entrance oriented to such plaza or parking/plaza area.

5. Building entrances shall be recessed or otherwise covered by pedestrian shelters, consistent with subsection 17.30.060(K).

6. Rear building entrances, and entrances facing a plaza or parking area that may also serve as a plaza or community gathering space during special events, shall incorporate patios with decorative landscape structures, such as garden walls, arbors, trellises, porticos, or pergolas with lighted pathways. Such patios and structures shall comprise not less than thirty (30) percent of the building frontage where it abuts the plaza/parking area.

FINDING: Subsections 2-6 are not applicable to the proposed structure. The building is an accessory structure to City Hall, which complies with the above provisions.

7. Off-street parking, trash pick-up, and above ground utilities, including but not limited to utility vaults and propane tanks, shall not be placed between building entrances and the street(s) to which they are oriented, but shall be oriented internally to the block, screened, and accessed by alleys to the extent practicable.

FINDING: Trash receptacles and the propane tank are not currently in the area subject to the SDR application. There are no plans to move them into this project area. All utilities (power and water if added) will be placed underground. Trash receptacles will be blocked from new parking area and away from new vehicle storage building.

8. Where off-street parking is provided, it shall conform to the dimensional standards and landscape standards of Chapter 17.68. 17.68.050, Off street parking requirements state that 1 spaces/300 s.f. plus one space /2 employees is required for Government buildings.

FINDING: The current City Hall building is 24 x72 feet or 1,728 square feet, requiring 6 parking spaces plus 3 spaces for the 5 staff members within City Hall. 9 parking stalls are required per code. The proposal shows 13 spaces plus the 7 existing spaces in front of City Hall.

9. Street access points, including new or modified driveway approaches, shall conform to the Access Control Standards in Section 17.100. Uses and development located outside the Downtown District shall not receive vehicle access through the Downtown District, except driveway approaches lawfully established prior to [Effective date of Downtown District] are permitted, subject to the requirements of Chapter 17.100.

FINDING: The current access for the parking lot accessing the proposed building is taken from 1st Street, which is a collector street per the City of Carlton Transportation Systems Plan (TSP). The proposed parking lot design shows two driveways deriving access from 1st Street. Per CDC 17.100.030, the minimum spacing between two driveways is 75 feet, not met by the proposed design. The centerline of the Northern driveway is within 50 feet on the existing access drive to the North of the property (servicing the dentist office and a single family home), and the proposed Southern access point. The applicant is requesting an **adjustment** to the access spacing standard to allow police vehicles to have access to the replacement vehicle storage building. Staff finds that due to the limited traffic accessing the driveway to the North, the access spacing adjustment complies with the intent of the ordinance. Permitting two entrances will also allow for safer circulation around the parking lot.

10. Where a development contains multiple buildings and there is insufficient street frontage to which buildings can be oriented, a primary entrance may be oriented to plaza, courtyard, or similar space containing pedestrian amenities. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a landscaped and lighted walkway with an approved surface not less than five (5) feet in width.

FINDING: The primary building, City Hall, currently has pedestrian access. Per the applicant, parking area is lit by downward facing streetlight-style light fixtures.

11. Buildings on corner lots shall have corner entrances or contain architectural features that emphasize the corner (e.g., chamfered/rounded edge, windows, molding, art).

FINDING: While the proposed storage building is located on a corner lot, it is an accessory structure and not subject to subsection 11.

12. Primary building entrances shall be at least fifty percent (50%) transparent so that two-way views, in and out of a building, are possible. This standard can be met by a door with a window, a transom window above the door, or sidelights beside the door. Where ATMs or kiosks are proposed, they shall be visible from the street for security and have a canopy, awning, or other weather protection shelter.

FINDING: This vehicle building is not the primary building on the property. The applicant is requesting an **adjustment** to the building transparency requirements of subsection 12 due to the needed security. Staff finds that the adjustment is warranted due to the security needs and off-street location of the structure.

F. Front Façade.

Materials used on the front façade shall turn the building corners and extend for a length of at least 12-inches across each side elevations to avoid the appearance of a false front building.

FINDING: The vehicle storage building will have the same material (metal) on all sides, meeting the standard.

G. Building Openings.

The following standards are intended to facilitate safe, direct, and convenient pedestrian access to buildings and uses, enhance the appearance of the downtown, and protect the historic integrity of the downtown. For the purposes of this Chapter, "transparent" means allowing two-way views in and out of a building. All of the following standards must be met, or adjustments approved, as applicable:

1. Architectural detailing shall define building entrances. Detailing may include, but is not necessarily limited to, a stoop or recess behind the front plane of the building, a canopy or awning cover, an entryway plaza (e.g., with pavers and seating), planter beds, window boxes, or similar detailing.

FINDING: Because the building will not be accessible to the public and is accessory to the primary building, the applicant is requesting an **adjustment** to subsection 1 for the security needs of the proposed building. Staff finds that this adjustment is warranted. Planning Commission may require additional detailing to be located on the front of the building as is feasible via an added condition of approval.

2. The ground floor, street- or plaza-facing elevation(s) of buildings located within one hundred (100) feet of Main Street shall comprise not less than sixty (60) percent transparent windows. Building abutting Main Street shall have windowed doors and transom windows, except where historical precedence dictates otherwise.

FINDING: The proposed building is located more than 100 feet from Main Street. Subsection 2 is not applicable.

3. All ground floor building elevations located more than one hundred (100) feet from Main Street and facing a street, plaza, or courtyard shall comprise not less than thirty (30) percent transparent windows.

FINDING: The new vehicle building is located more than 100 feet from Main street and will face 1st street. As discussed above, the building will not contain windows for security reasons. The applicant is requesting an **adjustment** to subsection. Staff finds this adjustment is warranted because windows would decrease the necessary security of the building.

4. All side building elevations not otherwise subject to the provisions of subsection 2 or 3, above, shall comprise not less than twenty (20) percent transparency; except zero-lot line/common wall elevations are not required to provide windows.

FINDING: As previously discussed, the building is accessory and will not contain windows. The applicant has requested an **adjustment** is requested to maintain the security of the vehicles and other items stored within the building. Staff finds this adjustment is warranted to maintain the security of the vehicles.

5. Window coverage shall be measured along the width of the street-facing elevation, between the building base (twenty-four (24) inches above the sidewalk grade, whichever is less) and a plane seventy-two (72) inches above the sidewalk grade.

6. The Planning Commission may grant exceptions (no adjustment required) to the window transparency standards for buildings containing industrial processing uses, provided the subject elevation contains detailing (e.g., false windows, offsets, projections, bays, changes in materials and/or texturing, or similar details) that break up the wall into smaller components.

FINDING: The building will not contain windows. An **adjustment** to subsections 5 and 6 are requested.

7. Windows shall contain trim, reveals or recesses of not less than four (4) inches in width or depth as applicable. The use of sills and decorative detailing and ornamentation around windows (e.g., patterning, corbels, medallions, pediments, shutters, or similar features), as appropriate for the sub-district, is required. This provision also applies to false windows.

FINDING: No windows are proposed for this building. The applicant is requesting an **adjustment** to the requirement of subsection 7.

8. Windows in the D-MS Sub-district. In the D-MS sub-district, upper story windows shall be vertically oriented, their height greater than their width. Upper story windows shall follow the vertical lines of the lower level piers and the horizontal definition of spandrels and any cornices. Paired or grouped windows that, together, are wider than they are tall, shall be visually divided to express the vertical orientation of individual windows. Except for transom windows and bay windows, windows and display cases shall not break the front plane of the building; projecting display boxes and bay windows are not allowed on elevations facing Main Street. For reasons of durability and historic compatibility, display cases, when provided, shall be flush with the building façade (not affixed to the exterior) and integrated into the building design with trim or other detailing. Window flower boxes are allowed provided they do not encroach into the pedestrian through-zone.

FINDING: Subsection 8 is not applicable.

9. Decorative wall-mounted lighting, consistent with the architectural of the building, shall be provided for signage and at primary entrances.

FINDING: No signage is proposed for this project.

H. Building Height Bonus (Option).

The following standards are intended to support the urban design objectives for downtown and facilitate mixed-use development through increased building height, while protecting the historic integrity of downtown buildings. All of the standards below must be met, or adjustments approved, as applicable, for approval of a building height bonus:

- 1. The maximum allowable height may be increased from thirty-five (35) feet to forty-five (45) feet on corner lots located within one hundred (100) feet of the following intersections, pursuant to subsections 2 and 3, below: Main Street/Pine Street, Main Street/Kutch Street, Main Street/Yamhill Street, and Pine Street/Grant Street.*
- 2. The portion of the building exceeding thirty-five (35) feet shall cover not more than thirty (30) percent of the building floor plate, as defined by the building foundation perimeter except the height increase may be extended to up to one hundred (100) percent of the floor plate for a mixed-use building where the upper story incorporates multifamily dwelling units or overnight accommodations (e.g., hotel) and civic space is provided pursuant to subsection 17.3.0.060(L).*
- 3. The portion of the building exceeding thirty-five (35) feet in height shall step-back from (recess behind) the building plane of the ground floor by at least four (4) feet.*

FINDING: Section H is not applicable to this application.

I. Building Line and Rhythm.

1. Horizontal Rhythm. Buildings facing a street or plaza must incorporate rhythmic divisions that relate to historic building patterns. Front elevations should be articulated (e.g., offset, recess, projection, or similar "break" in the wall plane) not less than once every twenty-five (25) feet. (This standard does not apply to building elevations that are less than fifty (50) feet in width.) Articulation should be subtle. For example, slight offsets in a building elevation, roofline and/or the rhythmic placement of windows, pilasters, awnings/canopies, trim, art/medallions, or other detailing and ornamentation can satisfy the standard. Changes in paint color do not satisfy this standard. Side and rear elevations may be articulated less frequently but should complement the overall building design. The Planning Commission may allow alternative detailing, such as a mural or landscape trellis where other detailing is impractical, such as on a zero-lot line elevation to reduce the apparent scale and avoid blank walls (i.e., until an abutting property develops). See examples in Section 17.30.070.

FINDING: The proposed building is 30x48 feet wide, less than the 50 feet necessary to be subject to the standard. The applicant has indicated that the North side of the building could contain additional detailing should the Planning Commission require it as a condition of approval.

2. Horizontal Lines. Building elevations within one hundred (100) feet of Main Street shall follow prominent horizontal lines existing on adjacent buildings at similar levels along the street frontage. Examples of such horizontal lines include but are not necessary limited to the base below a series of storefront windows; an existing awning or canopy line; a belt course between building stories; and/or an existing cornice or parapet line. See examples in Section 17.30.070. Exceptions: Where adjacent buildings do not provide a historically appropriate reference, the development may establish new horizontal lines consistent with historical precedence. See examples in Section 17.30.070.

FINDING: The proposed building is not within 100 feet of Main Street. Subsection 2 is not applicable.

3. Ground Floor/Upper Floor Division. Building elevations within one hundred (100) feet of Main Street shall maintain clear visual division between the ground level floor and upper floors; this is accomplished, for example, through the use of a belt course, transom, awnings or canopies in the D-MS sub-district, and through the use of wood trim and other detailing in the D-WG and D-RR sub-districts, consistent with historical precedence.

FINDING: The proposed building is not within 100 feet of Main Street. Subsection 3 is not applicable.

4. Vertical Rhythm. Buildings shall reflect a vertical orientation, through either actual volume, roof form, and/or the use of surface detail; for example, the use of vertically oriented sash windows and masonry trim in the D-MS sub-district, and pitched roofs and/or board and batten siding and corrugated metal detailing in the D-WG and D-RR sub-districts. See examples in Section 17.30.070.

FINDING: The proposed building will have metal walls and roof. The applicant has indicated that walls and roofs will be in board and batten style, meeting the criterion in subsection 4.

5. Roof Form.

a. D-MS: Predominate roof form shall be a flat roof with appropriately scaled cornice or stepped parapet top.

b. D-WG: Predominate roof form shall be pitched (4:12 minimum); gabled, hipped, modified pitched roof forms are allowed.

c. D-RR: No restriction on roof form provided the form reflects historic precedence (e.g., depot, ranch/farm buildings, silos/granary tower, historic main street, etc.) and is not an artificial or applied roof form.

FINDING: The subject property is zoned D-RR and is therefore not subject to roof form requirements. The roof is in character with the D-RR district.

J. Materials and Color.

City approval of proposed exterior materials and colors is required for all exterior remodels and alterations regardless of whether site review is required. The Planning Commission may approve adjustments to the following standards pursuant to Section 17.20.070.

1. Exterior Cladding.

a. D-MS: Except as permitted under subsection 'd', below, exterior cladding on new buildings shall predominately consist of durable reddish brick blends, as generally illustrated in Section 17.30.070; brick veneer is also permitted, except as otherwise required for designated historic buildings.

b. D-WG: Except as permitted under subsection 'd', below, exterior cladding of buildings shall predominately consist of wood or fiber cement (lap, panel, board and batten, shingle or similar siding), stucco, stone, textured concrete, split-face concrete block, and/or similar masonry.

Corrugated metal may be used as a secondary material only; vinyl siding and faux/cultured stone are not permitted.

c. D-RR: There is no restriction on exterior cladding, provided the design reflects historic precedence of the sub-district (e.g., wood lap, panel, board and batten, shingle, stucco, stone, split-face concrete block, corrugated or sheet metal, etc.) and does not include vinyl siding.

FINDING: The subject property is zoned D-RR and therefore not subject to material and color restrictions. The applicant has indicated that the building will be in the board and batten style to reflect historic precedence of the sub-district.

d. Building Additions: Building additions shall conform to the above standards or contain cladding similar to the original cladding of the structure. For purposes of this subsection, "original" means the cladding (material and detailing) used when the building was first constructed.

FINDING: No building additions are proposed.

e. Secondary Cladding. Any material permitted above may be used as secondary exterior cladding in the respective sub-district. Metals such as copper, steel, iron, bronze and similar appearance metals may be used as trims or accents (e.g., flashing, wainscoting, weather protection features, ornamentation, etc.) when non-reflective and consistent with historic precedence. In addition, rough-hewn wood, and timbers may be used as accents in the D-WG and D-RR sub-districts.

FINDING: The applicant has indicated that the future City Hall renovation will include timbers to meet the standard of subsection e. The City Hall project will include timbers and will meet this criterion. The metal siding of the building will be non-reflective.

2. Four Sides of Building. New buildings shall be designed so that all four sides contain complementary exteriors. Changes in material, texture or detailing (e.g., use of two or more different types of material) shall break up otherwise blank walls and define a building's base, middle and top. Side and rear elevations that do not face a street, plaza, patio, or pedestrian access way need not have two or more types of material where changes in texture or detailing break up the wall, consistent with the overall composition of the building.

FINDING: The front of the building is occupied by the four proposed roll-up doors. The applicant has indicated that the doors will complement the siding and roof colors. The front of this building is broken up by 4 large roll-up doors. The doors will complement both the siding and roof colors. As shown on page C03 of the site plan, the West side of the proposed building is screened by arbor vitae trees and the South side borders the second shop building, not requiring two types of materials. The North side of the structure will face the driveway on the neighboring property, and therefore also does not require two types of material.

3. Roofs.

a. Where pitched roofs are proposed, roof surfaces shall be wood, slate, cement tile, asphalt shingles, flat metal, or standing rib seam sheet metal.

FINDING: The applicant has indicated that the proposed building is rib seam sheet metal, meeting the criterion.

b. Metal roofing shall have a non-glare (e.g., matte finish).

FINDING: The exterior of the proposed storage building will have matte finish and therefore will not glare, meeting the criterion.

c. Where flat roofs are proposed, cornices and parapets shall incorporate materials that are consistent with historic precedence, as applicable, or otherwise consistent with the overall composition of the building.

FINDING: A flat roof is not proposed. Subsection c is not applicable.

d. Roofs must be non-reflective and light in color (e.g., light gray or ash, brown, or other earth-tone), and not clash with exterior cladding.

FINDING: The applicant has indicated that the roof will be light in color and match exterior cladding. A condition of approval has been added requiring the building to meet the downtown color palette. Staff will review the color for compliance prior to the issuance of a building permit.

4. Windows and Doors. All windows and doors must have wood or vinyl-coated wood trim, or masonry trim and sills. See also, Pedestrian Shelter requirements under subsection 17.30.060(K).

FINDING: The applicant is requesting an **adjustment** to allow for metal trim as opposed to vinyl. Due to the nature of the accessory structure, staff finds this request is appropriate.

5. Substitute Materials. The Planning Commission may approve an adjustment allowing substitute materials that are equal in appearance and durability to those listed above, provided such materials are historically appropriate. The applicant will be required to provide specifications from the manufacturer.

FINDING: Information and specifications from the manufacturer is included in Exhibit A4-Applicant's materials- Building Plans.

6. Color. Paint colors shall be consistent with the historic color palette as generally illustrated in Section 17.30.070 and on file at Carlton City Hall. Painting schemes shall be simple and coordinated over the entire building to establish a sense of overall composition. Reflective, luminescent, sparkling, and "day-glow" colors and finishes, and clashing paint colors or patterns are prohibited. Metals shall be matte finish, earth-tone color, or burnished/non-reflective (e.g., metal that has a non-reflective finish is allowed in the RR sub-district).

FINDING: As indicated above, the color of the building. will be reviewed for compliance with the downtown color palette. The building will be non-reflective.

K. Pedestrian Shelters.

Within the D-MS sub-district, awnings, canopies, recesses or similar pedestrian shelters shall be provided along at least sixty (60) percent of a building's ground floor elevation(s) where the building abuts a sidewalk or civic space (e.g., plaza). Within the D-WG and D-RR sub-districts, the minimum pedestrian shelter requirement is forty (40) percent.

FINDING: The proposed building does not abut a sidewalk or civic space. Therefore, Section K is not applicable.

L. Civic Space and Pedestrian Amenities.

1. Purpose. The City encourages the provision of civic space in new development through regulatory incentives. Civic space such as plazas, courtyards, patios, and expanded sidewalks/outdoor seating areas should be provided along street frontages and where gaps between buildings occur. Civic spaces can make the downtown more attractive and inviting while providing informal gathering places for rest and socialization.

2. Applicability. The following applies to site review proposals involving: (a) a height bonus pursuant to subsection 17.30.060(H); (b) an adjustment to a code standard under site review; or (c) where the applicant voluntarily provides civic space.

3. Civic Space Standards. At least three (3) percent, or not less than three hundred (300) square feet, of the site should be designated and improved as civic space (plaza, landscaped courtyard, sidewalk extension, or similar space). Such areas should be accessible to the general public, with the highest priority locations being those areas with the highest pedestrian activity. Civic spaces should be connected to a public right-of-way by a sidewalk or pedestrian access way. All civic spaces shall have dimensions that allow for reasonable pedestrian access. For example, a small site may provide a 4-foot wide strip adjoining and adding on to the sidewalk for a small café seating area, whereas a larger site at a street corner may provide a plaza adjacent to a building entrance. Civic spaces shall include pedestrian amenities, per subsection 4, below. See also, Chapter 17.84 Site and Landscaping Design.

FINDING: Because adjustments are requested, the development of a civic space is required under the provisions of subsection 2(b). Because the next phase of the City Hall project will be more oriented to pedestrian design, staff recommends the civic space standard be reviewed with the development occurring within the second phase.

4. Pedestrian Amenities Standards. Where street frontage improvements are required to comply with the Transportation System Plan, or where or civic space is required under subsection 17.30.60(L), street frontages shall be improved with pedestrian amenities such as benches, public art, pedestrian-scale lighting, shade structures, way finding signs, or similar pedestrian facilities in an amount equal to or greater than one-half of one percent (0.5%) of the estimated construction cost of the proposed building(s), subject to review and approval by the Planning Commission. Where a civic space adjoins a building entrance, the cost of providing a weather protection canopy, awning, arcade, overhanging eave, arbor, portico, or similar feature, consistent subsection 17.30.060(K), may be credited toward the one-half of one percent (0.5%) requirement. Pedestrian amenities such as seating, planters, public art and pedestrian lighting (e.g., street

lamps or pathway bollard lights) at street corners or paved mid-block pedestrian access ways between buildings may also be counted toward the one-half of one percent (0.5%) requirement. The cost of a proposed public-private parking facility may be subtracted from building costs used in the assessment of civic space improvements. A licensed architect, landscape architect, or other qualified professional, shall prepare cost estimates for civic space improvements, which shall be subject to review and approval by the Planning Commission.

FINDING: Street improvements are proposed along 1st Street, but there is limited space for pedestrian amenities. Staff recommends pedestrian amenities be included in the next phase of the City Hall redevelopment project.

M. Signs.

In addition to complying with requirements of Chapter 17.80 Signs, conformance to following standards is required in the Downtown district:

- 1. Building designs shall incorporate a sign band or otherwise provide for blade signs, awning signs, marquees, or other compatible sign types.*
- 2. Pole signs are prohibited.*
- 3. Monument signs shall not exceed six (6) feet in height and forty-eight (48) square feet of sign face for each side of a two-sided sign. A minimum of fifty (50) feet of street frontage is required for one monument sign.*
- 4. On multitenant buildings, signs shall be designed to accommodate multiple tenants.*

FINDING: No signage is proposed as part of the application.

N. Landscaping.

In addition to complying with requirements of Chapter 17.84 Site and Landscaping Design, conformance to following standards is required in the Downtown district:

- 1. All Downtown Sub-districts: Landscape designs must be compatible with the downtown, where buildings are generally placed closer together than in other districts. Designs take into consideration exposure to sun and wind, opportunities for pedestrian safety and comfort (e.g., buffering vehicle areas, summer shade, etc.), maintenance requirements, and downtown beautification objectives. These factors are to be balanced so that the resulting design is functional, attractive, and cost-effective. For example, landscaping must define pedestrian pathways, buffer parking lots from outdoor seating areas, break up large blank walls, and add color and interest to streetscapes, parking lots, and plaza areas.*
- 3. D-WG and D-RR: Ten (10) percent, minimum; may include required street trees (twenty-five (25) square feet per tree), planter beds, plant containers or window boxes, arbors, trellises, and climbing vines on garden walls and fences, as applicable. Where landscape structures such as arbors are proposed, the Planning Commission may count the total surface area to be covered by plants within two (2) years of planting.*

FINDING: The paved area of the parking lot is approximately 6468 square feet. The area to be landscaped is approximately 665 square feet, meeting the 10% standard. Proposed landscaping includes two types of trees and shrubs as indicated on page L01 of the site plan. The applicant has also indicated additional landscaping is planned with the future City Hall project.

4. Maintenance: All landscape and civic space areas shall be maintained and, as necessary, replaced by the property owner to ensure plant survival and upkeep of street furnishings, paving, and other built features. The use of water-conserving features, such small parking lot perimeter swales, and rain gardens fed by cisterns or roof drains is encouraged.

FINDING: All maintenance and upkeep of landscaped area will be done by the City. The applicant has indicated that water conservation features will be completed as part of the larger City Hall project.

O. Mechanical Equipment.

1. Building Walls. Mechanical equipment shall not be mounted on any building where it will be visible within one hundred (100) feet of Main Street. When mechanical equipment, such as utility vaults, air compressors, generators, antennae, satellite dishes, or similar equipment, must be installed on a rooftop or adjacent to a building wall, it shall be screened from view. Where such equipment is installed on a side or rear building elevation and is adjacent to a plaza, pathway, or other public space, it shall be screened in accordance with Chapter 17.84. Standpipes, meters, vaults and similar incidental equipment need not be screened but shall not be placed on a front elevation when other practical alternatives exist; such equipment shall be placed low on a side or rear elevation to the extent practical.

FINDING: No mechanical equipment will be used on the subject property. Section O is not applicable.

2. Rooftops. Except as provided below, rooftop mechanical units shall not be visible from the street or any alley, pedestrian access way or civic space. Such units should be screened behind a parapet wall or painted with muted, earth-tone colors that make them visually subordinate to their backgrounds. Exception: Equipment for small-scale renewable energy (e.g., mini-wind turbines, solar panels, and similar features) is allowed subject to site review; the Planning Commission may exempt such a facility from the screening standard upon finding that the screening would interfere with its operation, and the facility does not adversely impact any buildings of local historic significance.

FINDING: No mechanical equipment will be used on the subject property. Section O is not applicable.

3. Ground-Mounted Mechanical Equipment. Ground-mounted equipment (e.g., generators and air compressors) shall be limited to side or rear yards and screened in accordance with Chapter 17.84. The City may require additional setbacks and/or noise attenuating equipment to promote compatibility with adjacent uses.

FINDING: No ground mounted equipment is included in the proposal. The applicant has indicated that if a generator is needed, it will be located between the two shop buildings on the East side of the property and will be screened and buffered as needed. A condition of approval has been included to mandate adequate noise and visual buffering.

4. Plazas and Open Spaces. Mechanical equipment and garbage storage areas are not permitted within plazas or other public open spaces, except as approved with a conditional use permit. Where such facilities are allowed, the Planning Commission may require that such facilities be screened completely from view and set back from a civic space for aesthetic reasons and to minimize odors or noise.

FINDING: The garbage storage will not be moved from its current location, between City Hall and the police department buildings. It is not adjacent to a civic space.

P. Historic Building Alterations.

In addition to complying with requirements of Chapter 17.104 Historic Sites, conformance to following standards is required historic building alterations in the Downtown district:

1. Restoration, rehabilitation or remodeling projects shall incorporate, whenever possible, original design elements that have been removed, changed, or covered over.

FINDING: The current building is not considered historic and is not on any historic registers. Section P is not applicable.

17.64- Streets

FINDING: Per Exhibit B- Engineer's Comments: The subject property has frontage and access to both Main St. and 1st Street. 1st street is an existing collector street per the TSP. The requirements for existing collector streets are: 55' ROW, 40' pavement, curb & gutter, and a 6' sidewalk. The existing conditions are a 40' ROW, 18' pavement, no curb & gutter and no sidewalk. The proposed parking lot improvements and building replacement are infill development and street frontage improvements along the entire frontage of the property (along both the 1st Street and Main Street frontages) are required to comply with Section 17.64.E.2. The City Engineer recommends that street improvements only be required along the portion of 1st Street where the parking lot has frontage, and that additional improvements along the remainder of 1st Street and Main Streets be completed at the time of future improvements to City Hall to allow for coordinated development. To meet the provisions of 17.64.030, street improvements would be required as follows: 7.5' dedication of ROW, an additional 11' of pavement on the west side, curb & gutter and a 6' sidewalk. Due to limiting conditions (identified in points 1.c-f of Exhibit B), the City Engineer recommends that 5' sidewalk, curb & gutter and 25' of pavement be required to be installed as part of this project. A condition of approval is included requiring the recommended improvements.

Street Lights

FINDING: The subject development does not involve the creation of new streets or the extension of existing streets. Therefore, no new street lights are required.

17.64.070 Access easements

FINDING: CDC 17.64.070 does not apply because the applicant is not proposing to partition property or construct a private street or private access easement.

17.68 Off Street Parking, Loading and Bicycle Parking

17.68.030- Off-street parking and loading shall be located on the same lot with the main building or structure or use except that:

B. In any nonresidential zone, the parking area may be located off the site of the main building, structure or use if it is within five hundred (500) feet of such site.

FINDING: The parking lot is located on the same lot as City Hall, the primary building.

17.68.050- Off-Street Parking Requirements

G. Government buildings- One Space is required per 300 square feet plus one space for every two employees.

FINDING: The current City Hall building is 24 x72 feet or 1,728 square feet, requiring 6 parking spaces plus 3 spaces for the 5 staff members within City Hall. 9 parking stalls are required per code. The proposal shows 13 spaces plus the 7 existing spaces in front of City Hall.

17.68.080- Parking and Loading Area Requirements

A. Surfacing: all driveways, parking, and loading areas shall have a durable, hard surface.

B. Parking spaces: parking spaces shall be a minimum nine feet wide and eighteen (18) feet in length.

C. Driveways: The following driveway dimensions shall apply (See code)

D. Areas used for parking and maneuvering of vehicles shall be drained as to avoid flow of water across sidewalks.

E. Except for parking to serve residential uses, parking and loading areas adjacent to residential zones or adjacent to residential uses shall be designed to minimize disturbance of residents.

F. Groups of more than four parking spaces shall be so located and served by a driveway that their use will require no backing movements or other maneuvering within a street right-of-way other than an alley.

FINDINGS: All driveways shall be paved with a hard, durable surface. Driveways meet the width- The south drive aisle is 24 feet, expands to 26 feet on the West side, and is 20 feet on the North side of the parking lot. The parking lot will be drained to the satisfaction of the City Engineer. A landscape strip is proposed to the North of the property and the parking lot improvements improve the aesthetic quality and will add no additional noise or traffic to negatively impact residents to the North.

17.68.100- Parking Lot Landscaping and Screening Standards

A. Lighting. Any light used to illuminate parking or loading area shall be arranged to be directed entirely onto the loading or parking area, shall be deflected away from any residential use and shall not cast a glare or reflection onto moving vehicles on the public rights-of-way.

FINDING: The applicant has indicated that all lighting shall be from above and directed only onto the parking area. No light trespass is permitted.

B. Screening Abutting Property Lines. Parking for commercial, industrial and multifamily uses which abut a residential use or zone property line shall be screened by a five-foot landscaped

strip. Where a buffer between zones is required, the screening should be incorporated into the required buffer strip, and will not be an additional requirement. The screen shall grow to be at least thirty-six (36) inches higher than the finished grade of the parking areas, except for required vision clearance areas.

FINDING: As the existing use is a public use, Subsection B does not apply. The applicant is proposing a 5 foot landscape strip along the perimeter of the parking area.

C. Landscape Standards. Landscaping within or adjacent to a parking lot shall consist of a minimum of ten (10) percent of the total parking area plus a ratio of one tree per ten (10) parking spaces. Trees and landscaping shall be installed as follows: (See code for specifics)

FINDING: The paved area of the parking lot is approximately 6468 square feet. The area to be landscaped is approximately 665 square feet, meeting the 10% standard. Proposed landscaping includes two types of trees and shrubs as indicated on page L01 of the site plan.

D. Wheel Guards. Parking lot landscaping shall be protected from damage by a secured wheel guards to prevent vehicles entering into landscaped areas.

FINDINGS: Wheel guards have been provided for the parking spaces against City Hall. No other wheel guards are required as parking spaces do not front any of the landscaped areas.

17.72 - Storm Drainage and Erosion Control

FINDING: Per Exhibit B- Engineer's Comments:

Storm Drainage: Currently, the storm drainage from the parking lot is collected through area drains connected to an 8" concrete pipe that discharged to an existing storm ditch at the intersection of First St. and E. Market St. The ditch enters a culvert under a driveway. The pipe connects to a pipe on Second St. which extends to E. Main St. and eventually discharges to Hawk Creek.

The proposed storm drainage utilizes the same system using two area drains in new locations in the parking lot, and a catch basin connected to the existing 8" pipe. As this is draining the same area which is currently either pavement, building or crushed rock it is reasonable to expect that there would be marginal or no additional flow.

During the design phase a storm drainage report should be conducted to verify the pre and post development storm flow, and the capacity of the ditch. If the ditch has insufficient capacity, then additional storm facilities may be required. This could come in the form of ditch capacity improvements, low impact development storm system improvements, or detention. A condition of approval has been added to reflect this recommendation.

17.76 - Utility Lines and Facilities

Water System

FINDING: Per Exhibit B- Engineer's Comments:

There is a 2" water line on N. First St. that extends from E. Monroe St. to approximately the mid-point of the proposed parking lot. There is a fire hydrant at the intersection of N. First St. and E. Main St. that has sufficient fire flow for the City Hall. While the water line does not extend across the frontage of the property, there is already water service to the subject property and every tax lot along the frontage of the subject property. There is no need to extend the water line to provide service or facilitate further development to adjacent properties. Fire flow is provided to the property from the fire hydrant on E. Main Street. There may be distance issues to the back of the property. A 2" water line is undersized and should be upgraded at some point. The City has a CIP project projected to be within the next five years that would extend a new water line on First St. from E. Main St. to E. Monroe St., which would include an additional fire hydrant at the intersection of First St. and E. Market St. As the existing water service is adequate, and the City has a CIP project to address upsizing the water line in the near future, it is recommended that no public water line improvements would be required at this time. Each lot is required to have an individual water service. The subject property already has water service, so no improvement would be required.

Sewer System

FINDING: Sanitary Sewer: There is an 8" sanitary sewer pipe along the entire property frontage on E. Main St. that provides sanitary sewer service to the property. There is no sanitary sewer on First St. All properties along First St. between E. Main St. and E. Monroe St. already have sanitary sewer service, so there is no need to extend the main line in this location.

Sanitary Service: Each lot would be required to have an individual sanitary service. There is existing service to the property, so no improvement is required.

17.80 - Signs

FINDING: No signage is proposed as part of this application. As required by CDC Section 17.80, The applicant will need to submit a sign permit prior to installing any new signage.

17.84 - Site and Landscaping Design (Including Screening and Buffering)

FINDING: Findings related to site landscaping requirements with the D-RR zone may be found under Section N above. The submitted landscape plan meets the requirements of Section 17.84.

17.88 - Development Standards For Land Divisions

FINDING: CDC 17.88 is not applicable since no land division is proposed.

17.92 - Yards (Including Fences, Walls and Hedges)

FINDING: There are no required yards for the subject property. The proposed building will replace the existing building in the same location.

17.96 - Accessory Structures

FINDING: The vehicle storage building is considered an accessory structure. Per Section 17.96.030.A-E, there are no limits to the number of accessory structures within the downtown District and the building height and size complies with the standards of the D-RR zone as discussed in the findings for section 17.30. Design standards for building are also addressed in the findings for Section 17.30 above.

17.100 - Access Control Standards

FINDING: Driveways are required to be separated by 75 feet for collector streets. The proposed driveway on the Northern side of the parking lot is currently used as a driveway and is unmarked. The proposal creating a designated driveway is an improvement and is necessary for circulation and maneuvering around the parking lot.

17.104 - Historic Sites

FINDING: The storage building is not considered historic and is not on the historic registry.

17.156.060(B) Application of the standards of this title relative to the characteristics of adjoining and surrounding uses;

FINDING: The only impact to the surrounding uses is the addition of the access drive on the Northern portion of the parking area. Per the submitted site plan, the driveway will be entrance only.

17.156.060(C) Drainage and erosion control requirements relative to applicable city and DEQ standards, and engineering best practices;

FINDING: See findings for storm drainage in section 17.72 above.

17.156.060(D) Public health factors relative to applicable building codes, sanitation requirements, and city standards;

FINDING: The proposed building will require a building permit and will need to meet all local and state building code requirements. The proposed alterations and constructions have no impact on sanitation requirements or other City standards.

17.156.060(E) Parking, traffic safety, and connectivity of internal circulation to existing and proposed streets, bikeways and pedestrian facilities.

FINDING: Adequate parking is provided as part of the application in compliance with the provisions of CDC Chapter 17.68. 5 foot sidewalks will be added along the portion of 1st Street parallel to the proposed parking area improvements. Future pedestrian facilities will be required at the time of the future renovations to City Hall.

17.156.060(F) Provision for adequate noise and/or visual buffering from non-compatible uses;

FINDING: There are no non-compatible uses adjacent to the subject property. The applicant has provided the required 10% site landscaping as required per Chapter 17.84.

17.156.060(G) Balancing retention of natural features with individual property rights and growth management policies; and

FINDING: There are no natural features on the subject property.

17.156.060(H) Avoidance of natural hazards.

FINDINGS: No known hazard areas are located on or near the subject property.

V. RECOMMENDATION

Major Site Design Review (SDR 2021-01) Recommendation

Staff recommends approval of Major Site Design Review 2021-01 subject to the following conditions:

All conditions of approval required by the city for a Type II or Type III approval shall be completed prior to the issuance of building permits, except that the Planning Commission may stipulate that some conditions be completed prior to issuance of building occupancy permits when the former is impractical. When an applicant provides information demonstrating to the satisfaction of the Planning Commission that it is not practical to fulfill all conditions prior to issuance of a building permit, the applicant must complete a performance guarantee for required improvements pursuant to Chapter 17.216.

1. Site Design Review **approval shall be effective** for a period of eighteen (18) months from the date of approval. Site Design Review approval shall be voided immediately if construction on the site is a departure from the approved plan. If substantial construction of the approved plan has not begun within the eighteen month period, the approval shall expire. Except that the City Manager shall upon written request by the Developer and Property Owner and payment of the required fee, grant one extension of the approval for a period not to exceed six (6) months provided that:
 - a. No changes are made to the approved Site Design Plan;
 - b. The Developer and Property Owner can show intent to initiate construction on the site within the six-month extension period;
 - c. There have been no changes in existing conditions, facts, or applicable policies or ordinance provisions on which the original approval was based; and
 - d. The request for extension is submitted, in writing, 30 days prior to the expiration of the approval period.
2. Compliance with the downtown color palette must be reviewed and approved by staff prior to the issuance of a building permit.
3. Future placement of a generator shall be located between the two shop buildings as indicated. Screening and noise buffering shall be provided to the satisfaction of staff prior to installation.

4. The developer shall complete street improvements along the frontage of the parking lot as recommended by the City Engineer. The final layout of the sidewalk or walkway system shall be acceptable to the City Engineer and Public Works.
5. If improvements covered under the PWDS are required, the developer and his engineer shall schedule and participate in a pre-engineering conference (per PWDS 1.9.a) with the City and any other public facility owners or utility providers for the purpose of coordinating any required street & utility improvements, as well as other agencies with jurisdiction over affected streets or utilities. This conference shall occur prior to submitting construction drawings for review by Public Works. Participants shall include City Public Works and the City Engineer, as well as the ODOT and/or franchise utility providers as applicable.
6. During the design phase, a storm drainage report shall be conducted to verify the pre and post-development storm flow, and the capacity of the ditch. If the ditch has insufficient capacity, then additional storm facilities shall be required to the satisfaction of the City Engineer and Public Works Department.

VI. PLANNING COMMISSION ACTION OPTIONS

A. Approve Major Site Design Review 2021-01;

1. As recommended by staff; and based upon the staff report and in compliance with the conditions of approval, or

2. As determined by the Planning Commission stating how the application satisfies all the required criteria, and any revisions to the recommended conditions of approval, or

B. Deny the major site design review application (stating how the application does not meet the required standards).

C. Continue the hearing to a time certain or indefinitely (considering the 120-day limit on applications).

VII. APPEALS

Either the applicant or persons receiving notice of the decision may appeal a Type II land use decision to the City Council. The appeal shall be filed within twelve (12) days from the date of the decision, pursuant to the provisions of Chapter 17.204.



This page intentionally left BLANK.



City of Carlton Site Design Review Application for Vehicle building

Location: 191 East Main street
 Tax Lot #: R3421AD 02800
 Zone: Downtown Railroad district (DD-RR)
 Comp Plan Designation: Public facility (PF)

Project description and background

On the City Hall property there are 2 storage/shop buildings. One is storage for both the City Hall and Police Departments. This shop is lighted, heated and has water/sewer to it. The cities permanent records are stored here due to the climate-controlled environment. Being the newest shop on the property, this building is in decent shape. This building will stay as is.

The other shop only has electricity to it. The roof and gutters leak due to rust, and the side walls also have random rust holes throughout all walls. The sliding doors are barely operational, and Officers have to wear gloves to slide the doors open due to the poor shape of the lumber on holding the doors together. There have been several accident reports filed by Officers for cuts, deep slivers, or abrasions from trying to open the doors.

Due to the poor shape of the doors, quotes for new doors were gathered by staff. Council approved the purchase of new roll up doors. It was at this meeting that Council asked staff to look into collecting quotes on a new building and to redo the roof and siding. After reviewing and comparing the cost between a new building and remodeling the current, Council opted to replace the whole building with one of the same size, with roll up doors and electricity. The new building will not have water or sewer connected. It will be used for the same purpose as the current building which is vehicle storage for the Police Department.

This building, just like the Police Department will be a secure facility. Officers store public safety items in their vehicles, as well as computers that have access to files and records. Windows and entrances to the building are limited because of these security reasons. The public will be restricted from using or accessing this building to keep the officers, their vehicles and equipment safe and operational. This building will be built with the same materials as the current shop, and will look like a shop, not an inviting building that the public could get mixed up for City Hall. It will look distinctly like a shop building for security reasons as well.

17.30.020- Permitted uses

K. Public and/or institutional uses, including public parking; and new on-premise parking subject to a public shared parking agreement.

- The building is for institutional uses, Police department vehicle parking and storage. Meeting “K” of this code.

17.30.030- Conditional uses

Does not pertain.

17.30.040- Prohibited uses and uses permitted with limitation

5. Shared parking and open space areas may be used to satisfy the requirements of subsections 17.30.040(C)(3) or 17.30.040(C)(4) where such areas are improved to city standards and allow public access, either through dedication to the city or through a public access easement with a maintenance agreement. Examples of open space areas include plazas, outdoor seating/dining areas, convertible plazas that may be used for parking or special events/community gatherings, and similar uses.

- While a new building is proposed to replace the current building, the open space in front is budgeted to be upgraded with new asphalt, drainage, sidewalks and parking. This will allow for both staff and public parking.

17.30.050- Dimensional Standards

Minimum lot area- none

- Proposal meets

Minimum Front/Street Side Yards- D-RR: None, except setback area provided must be landscaped or serve as an extension of the sidewalk (e.g., outdoor café or plaza), except where access drives/alleys preclude landscaping.

- Proposal meets

Minimum Rear Yard- None, except 15 feet when abutting a residential district or city-owned plaza or parking lot.

- Proposal meets; rear setback abuts Downtown zoning

Minimum Side Yard- None, except 10 feet required when abutting a residential district or city-owned plaza or parking lot, and as required for Street Side Yards.

- Proposal meets; the northwest corner of the property is 11 feet from the closest residential property, which is to the northwest. The south side does not abut a residential zone.

Minimum and Maximum Structure Height- D-RR: 35 feet, except existing grain elevator is permitted and may be rebuilt in current location to original height.

- Proposal meets; structure height is 14 feet.

17.30.060- Development and design standards**D. Design Guidelines**

1. Does the proposal contribute to the attributes that make the subject downtown sub-district (D-MS, D-WG, and/or D-RR) distinct? For example, does the proposal respond to the building forms, scale, setbacks, orientation, architectural style, materials, detailing, color, signage, parking, and/or other elements that distinguish the sub-district without mimicking other building designs? Is the proposal compatible with existing structures that have retained their historic integrity? It is not the City's intent to create an architectural theme, but rather to ensure that new buildings and alterations fit the historic context.

- This proposal contributes by removing a structure that is currently rusting and falling apart with a new structure that is the exact same size as the existing. The utilitarian style and function of the building will be the same as the current structure being replaced.

2. *If the proposal involves remodeling a building listed on a local state, or national historical register, is the remodel consistent with the guidelines for altering such historic resources?*

- Building is not on a historical record or register. Does not apply.

3. *Does the proposal enhance the streetscape or other public spaces with appropriate building placement, orientation, height, architectural detailing and landscaping?*

- The new structure will replace the current dilapidated building and enhance streetscape with a cleaner view. Building placement and orientation allows for vehicles to maneuver in and out of the building while making space for a much-needed public parking in the downtown area.

4. *If located at the intersection of Main Street/Pine Street, Main Street/Yamhill Street, or Pine Street/Grant Street, does the design enhance the "gateway" location with a corner plaza or vertical building elements (e.g., increased height) at the corner?*

- Not applicable.

5. *Does the structure have a compatible building scale relationship with adjacent residences? For example, does the structure step-down in height adjacent to single family dwellings?*

- The new structure is in scale with surrounding residences and buildings. Adjacent, there is a two-story dental office, and a two-story home. It is also the same height as the other shop structure on the property that is located four feet to the South.

6. *Does the building design address all four sides of the building with a unified design? For example, are the materials, textures and colors on each elevation coordinated?*

- The exterior of the building will be covered in sheet metal, including the roof. The color of the metal will meet the downtown design color palette. All walls will be the same color and material, and the roof will all be metal. It is not determined at this time if the roof will be a different color or the same color as the walls. Any and all color selections will meet the downtown design color palette options.

7. *Where a zero-setback is proposed, does the side elevation facilitate common wall development in the future?*

- Zero setbacks are proposed. Any additions (if any) to the structure in the future would be towards the south where the existing storage structure is standing. No development or additions would be made towards the North.

8. *Where the proposal includes an adjustment to the window transparency standards or other detailing standards, does the proposed design adequately break up the building elevation (avoid creating a blank wall) and express storefront character in other ways?*

- The proposed structure does not include any windows, only large roll up doors to break up the wall facing 1st street. The rear wall will not have any windows, nor the side walls as this is a utilitarian storage building. An exemption is request for window allowance due to the security and use of this building.

9. *Does the building contain openings (doors or windows) adjacent public spaces or parking areas, including those that may also serve as plazas or community gathering places during special events?*

- The building will have doors adjacent to the public parking area. Due to the security of the building for police department use, the building will not be accessible to the public. This is also the reason for limited openings. Only the parking area will be accessible to the public for parking purposes. To use the parking lot for special events, a special event permit will need to be applied for.

10. *Do the facade and roofline have a rhythm that is consistent with adjacent buildings, or appropriately transition from one building to another?*

- The roofline is consistent and simple. It does align with the surrounding buildings. The siding and roofing of the new building will be the same type and kind of the building four feet away. The only difference will be color. The color of the new building will meet the downtown design standards.

11. *Does the proposal contain adequate sidewalks? Sidewalks must contain a sufficient pedestrian through zone (clearance) and Americans with Disabilities Act accessibility. Sidewalks within the MS sub-district must also contain a furnishing zone, per Section 17.30.070.*

- Proposal is not in MS zone, no furnishing zone is required. Sidewalks will be placed on 1st street along frontage of parking area. Alley/driveway along north side of property will not have a sidewalk added since it is not public right of way. ADA compliance sidewalks will be installed where required.

12. *If located adjacent to a plazas or area with curb extension or widened sidewalk, does the proposal include benches, café seating, or public art, per Section 17.30.070?*

- Project is not located adjacent to a plaza or area with curb extension. No benches will be installed at this time.

13. *If the proposal adjusts the lot coverage standard or contains on-premise parking, does it manage storm water drainage more effectively than would be possible under a conventional design? Does it utilize on-site retention with water quality features?*

- On premise parking will be added. Storm water drainage will be steered toward a recently repaired existing storm drain near the proximity of the parking lot addition. It is located at the edge of the property along 1st street. This catch basin is currently being used for the existing parking lot. New parking area will not be any larger, capacity will not change or increase.

14. *Does the proposal promote water conservation, for example, through drought-tolerant plantings or capturing rainwater for use in landscape irrigation?*

- Any flowerbeds proposed in the project plan will include drought tolerate plantings. Rainwater collection will not be done with this project due to a lack of space. Irrigation sprinklers will be added due to the trees in the landscaping plan. The addition of drought tolerant plantings can also be added.

15. *If the proposal leaves a gap between buildings (non-common wall development), does it provide landscaping (e.g., courtyard garden), or a plaza, with seating in that area?*

- Does not apply to this build.

E. Building Orientation

1. *Comply with the dimensional standards in subsection 17.30.050.*

- This structure complies with 17.30.050 dimensional standards. See above.

2. *Provide at least one primary building entrance facing an abutting street (i.e., within 45 degrees of the street property line); or if the building entrance is turned more than 45 degrees from the street (i.e., front door is on a side elevation), the primary entrance shall open onto a pedestrian plaza or courtyard and a walkway shall connect the primary entrance to the plaza and sidewalk.*

- The building will not be for public use, only police officers and approved city staff. The primary doors to the building are the large roll vehicle doors. These doors will be facing 1st street and the parking area. Building will not be turned more than 45 degrees. It will be parallel with 1st street, just as the current building is.

3. *Development abutting Main Street shall orient to Main Street. Where a development does not abut Main Street, it shall provide a primary entrance facing the street that is likely to have the most pedestrian traffic, as determined by the Planning Commission.*

- Building does not abut Main street. 1st street has the most pedestrian traffic and the building will face it. Primary entrances (roll up doors) face this street. This building is not accessible to the public.

4. *Buildings on lots abutting a plaza or a parking/plaza area shall have at least one secondary entrance oriented to such plaza or parking/plaza area.*

- All four entrances (roll up doors), are oriented to the parking area. This building is not accessible to the public.

5. *Building entrances shall be recessed or otherwise covered by pedestrian shelters, consistent with subsection 17.30.060(K).*

- Building entrances (roll up doors) are for city staff only. An exemption is requested for this due to no public access and there is no man door, only roll up doors.

6. *Rear building entrances, and entrances facing a plaza or parking area that may also serve as a plaza or community gathering space during special events, shall incorporate patios with decorative landscape structures, such as garden walls, arbors, trellises, porticos, or pergolas with lighted pathways. Such patios and structures shall comprise not less than thirty (30) percent of the building frontage where it abuts the plaza/parking area.*

- A parking area is planned with this project, but the size is limited. The addition of patios or other structures would eliminate vital parking spaces near the downtown corridor that is already short on parking. A decorative element matching the zone and character of Carlton can

be added with the larger City Hall project and match the main building better. Some landscaping is proposed with this project.

7. Off-street parking, trash pick-up, and above ground utilities, including but not limited to utility vaults and propane tanks, shall not be placed between building entrances and the street(s) to which they are oriented, but shall be oriented internally to the block, screened, and accessed by alleys to the extent practicable.

- Trash receptacles and the propane tank are not in this area as of now, and there are no plans to move them into this project area. All utilities (power and water if added) will be placed underground. Trash receptacles will be blocked from new parking area and away from new vehicle storage building.

8. Where off-street parking is provided, it shall conform to the dimensional standards and landscape standards of Chapter 17.68.

- *17.68.050, Off street parking requirements state that 1 spaces/300 s.f. plus one space /2 employees is required for Government buildings.* The current City Hall building is 24 x72 feet or 1,728 square feet. There would be 6 parking stalls required due to square feet, plus an additional 3 parking spots for 5 staff working in the building. 9 parking stalls would be required per code. The design adds 13 spaces plus the 7 existing parking stalls in the front of City Hall.

9. Street access points, including new or modified driveway approaches, shall conform to the Access Control Standards in Section 17.100. Uses and development located outside the Downtown District shall not receive vehicle access through the Downtown District, except driveway approaches lawfully established prior to [Effective date of Downtown District] are permitted, subject to the requirements of Chapter 17.100.

- The current access for the vehicle building and current parking lot is off of 1st street. This will continue to be the access point. Currently, 1st street is a local street. The parking lot design plan shows an entrance and exit, both off of first street and similar to the current set up. First street has been recommended for an upgrade to a collector classification in the future. A local street has a minimum space between driveways and streets of 50 feet. A collector is 75 feet.
- Due to the Police department needing to exit out of the new vehicle storage building, an exemption is requested for driveway spacing. There is a driveway along the North property line currently servicing the dentist office staff and a single-family home only. The proposed parking lot design allows Police, (emergency responders), gives them quick access through the parking area, while still allows for ample room for easy parking inside the building.

10. Where a development contains multiple buildings and there is insufficient street frontage to which buildings can be oriented, a primary entrance may be oriented to plaza, courtyard, or similar space containing pedestrian amenities. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a landscaped and lighted walkway with an approved surface not less than five (5) feet in width.

- The main building, City Hall, has its entrance facing East Main street which contains a wide sidewalk for pedestrian use. There is parking in front of this building. The new vehicle structure will not be accessed by the public, but the new parking area will be. This area will be lit with a

downward facing light, similar to streetlights over the parking area. These lights will guide pedestrians from the parking lot to the street and vice versa.

11. Buildings on corner lots shall have corner entrances or contain architectural features that emphasize the corner (e.g., chamfered/rounded edge, windows, molding, art).

- The new structure will not be located on a street corner.

12. Primary building entrances shall be at least fifty percent (50%) transparent so that two-way views, in and out of a building, are possible. This standard can be met by a door with a window, a transom window above the door, or sidelights beside the door. Where ATMs or kiosks are proposed, they shall be visible from the street for security and have a canopy, awning, or other weather protection shelter.

- This vehicle building is not the primary building on the property, City Hall is. An exemption is requested for this criterion in order to maintain a high level of security for the officers and the equipment inside.

F. Front Façade.

Materials used on the front façade shall turn the building corners and extend for a length of at least 12-inches across each side elevations to avoid the appearance of a false front building.

- The vehicle building will have the same material (metal) on all sides which will meet this criterion and avoid the appearance of a false front building.

G. Building Openings.

The following standards are intended to facilitate safe, direct, and convenient pedestrian access to buildings and uses, enhance the appearance of the downtown, and protect the historic integrity of the downtown. For the purposes of this Chapter, "transparent" means allowing two-way views in and out of a building. All of the following standards must be met, or adjustments approved, as applicable:

1. Architectural detailing shall define building entrances. Detailing may include, but is not necessarily limited to, a stoop or recess behind the front plane of the building, a canopy or awning cover, an entryway plaza (e.g., with pavers and seating), planter beds, window boxes, or similar detailing.

- As this building will not be accessed by the public and for authorized city staff only, an exemption for security of the staff and equipment inside.

2. The ground floor, street- or plaza-facing elevation(s) of buildings located within one hundred (100) feet of Main Street shall comprise not less than sixty (60) percent transparent windows. Building abutting Main Street shall have windowed doors and transom windows, except where historical precedence dictates otherwise.

- This is a one-story building that is located over 100 feet from Main street- criterion does not apply.

3. All ground floor building elevations located more than one hundred (100) feet from Main Street and facing a street, plaza, or courtyard shall comprise not less than thirty (30) percent transparent windows.

- The new vehicle building is located more than 100 feet from Main street and will face 1st street. An exemption is requested for this criterion due to having 4 large roll up doors facing 1st street and there be little room for windows on the street facing wall.

4. All side building elevations not otherwise subject to the provisions of subsection 2 or 3, above, shall comprise not less than twenty (20) percent transparency; except zero-lot line/common wall elevations are not required to provide windows.

- For building and officer safety, this building will not contain any windows, only four large roll up doors. An exemption is requested in order to maintain officer safety and equipment security.

5. Window coverage shall be measured along the width of the street-facing elevation, between the building base (twenty-four (24) inches above the sidewalk grade, whichever is less) and a plane seventy-two (72) inches above the sidewalk grade.

6. The Planning Commission may grant exceptions (no adjustment required) to the window transparency standards for buildings containing industrial processing uses, provided the subject elevation contains detailing (e.g., false windows, offsets, projections, bays, changes in materials and/or texturing, or similar details) that break up the wall into smaller components.

- The street facing portion of the building contains four large roll up doors. The South side faces another existing shop building, the North faces the residential area and the rear faces a fence and arborvitae. Only the North side and street facing sides are the only sides that will be seen by others. The North wall is the only side without windows or doors to break up the wall. For security and safety reasons, the building needs to have limited entrances due to the nature of the items that could be in the vehicles. An exemption is requested for this.

7. Windows shall contain trim, reveals or recesses of not less than four (4) inches in width or depth as applicable. The use of sills and decorative detailing and ornamentation around windows (e.g., patterning, corbels, medallions, pediments, shutters, or similar features), as appropriate for the sub-district, is required. This provision also applies to false windows.

- No windows are proposed for this building. An exemption is requested for all window criteria due to the explanation above.

8. Windows in the D-MS Sub-district. In the D-MS sub-district, upper story windows shall be vertically oriented, their height greater than their width. Upper story windows shall follow the vertical lines of the lower level piers and the horizontal definition of spandrels and any cornices. Paired or grouped windows that, together, are wider than they are tall, shall be visually divided to express the vertical orientation of individual windows. Except for transom windows and bay windows, windows and display cases shall not break the front plane of the building; projecting display boxes and bay windows are not allowed on elevations facing Main Street. For reasons of durability and historic compatibility, display cases, when provided, shall be flush with the building façade (not affixed to the exterior) and integrated into the building design with trim or other detailing. Window flower boxes are allowed provided they do not encroach into the pedestrian through-zone.

- Does not apply. Property is in zone DD-RR.

9. Decorative wall-mounted lighting, consistent with the architectural of the building, shall be provided for signage and at primary entrances.

- No signage is proposed for this project.

10. *The Planning Commission may require security lighting at rear entries.*

11. *The Planning Commission may grant an exception to the window transparency requirement for parking garages, provided the building design shall incorporate openings, screening, or other detailing, subject to site review.*

H. Building Height Bonus (Option).

The following standards are intended to support the urban design objectives for downtown and facilitate mixed-use development through increased building height, while protecting the historic integrity of downtown buildings. All of the standards below must be met, or adjustments approved, as applicable, for approval of a building height bonus:

1. *The maximum allowable height may be increased from thirty-five (35) feet to forty-five (45) feet on corner lots located within one hundred (100) feet of the following intersections, pursuant to subsections 2 and 3, below: Main Street/Pine Street, Main Street/Kutch Street, Main Street/Yamhill Street, and Pine Street/Grant Street.*

- Not applicable.

2. *The portion of the building exceeding thirty-five (35) feet shall cover not more than thirty (30) percent of the building floor plate, as defined by the building foundation perimeter except the height increase may be extended to up to one hundred (100) percent of the floor plate for a mixed-use building where the upper story incorporates multifamily dwelling units or overnight accommodations (e.g., hotel) and civic space is provided pursuant to subsection 17.3.0.060(L).*

- Not applicable.

3. *The portion of the building exceeding thirty-five (35) feet in height shall step-back from (recess behind) the building plane of the ground floor by at least four (4) feet.*

- Not applicable.

I. Building Line and Rhythm.

1. *Horizontal Rhythm. Buildings facing a street or plaza must incorporate rhythmic divisions that relate to historic building patterns. Front elevations should be articulated (e.g., offset, recess, projection, or similar "break" in the wall plane) not less than once every twenty-five (25) feet. (This standard does not apply to building elevations that are less than fifty (50) feet in width.) Articulation should be subtle. For example, slight offsets in a building elevation, roofline and/or the rhythmic placement of windows, pilasters, awnings/canopies, trim, art/medallions, or other detailing and ornamentation can satisfy the standard. Changes in paint color do not satisfy this standard. Side and rear elevations may be articulated less frequently but should complement the overall building design. The Planning Commission may allow alternative detailing, such as a mural or landscape trellis where other detailing is impractical, such as on a zero-lot line elevation to reduce the apparent scale and avoid blank walls (i.e., until an abutting property develops). See examples in Section 17.30.070.*

- This vehicle building is less than 50 feet wide. The building size is 30x48 feet. The building will be flanked by the property line to the North, fence and arborvitae to the rear and another shop

building to the South. The North wall would be the only wall accessible to add any alternative detailing to. Alternatives can be considered and completed be if Planning Commission requires.

2. Horizontal Lines. Building elevations within one hundred (100) feet of Main Street shall follow prominent horizontal lines existing on adjacent buildings at similar levels along the street frontage. Examples of such horizontal lines include but are not necessary limited to the base below a series of storefront windows; an existing awning or canopy line; a belt course between building stories; and/or an existing cornice or parapet line. See examples in Section 17.30.070. Exceptions: Where adjacent buildings do not provide a historically appropriate reference, the development may establish new horizontal lines consistent with historical precedence. See examples in Section 17.30.070.

- Vehicle building is not within 100 feet of Main street. Does not apply.

3. Ground Floor/Upper Floor Division. Building elevations within one hundred (100) feet of Main Street shall maintain clear visual division between the ground level floor and upper floors; this is accomplished, for example, through the use of a belt course, transom, awnings or canopies in the D-MS sub-district, and through the use of wood trim and other detailing in the D-WG and D-RR sub-districts, consistent with historical precedence.

- Zone is DD-RR and due to the metal construction and fabrication of the building, wood trim and detailing may be difficult to add. Metal siding will mimic board and batten siding commonly used in turn of the century agriculture buildings.

4. Vertical Rhythm. Buildings shall reflect a vertical orientation, through either actual volume, roof form, and/or the use of surface detail; for example, the use of vertically oriented sash windows and masonry trim in the D-MS sub-district, and pitched roofs and/or board and batten siding and corrugated metal detailing in the D-WG and D-RR sub-districts. See examples in Section 17.30.070.

- Building will use metal walls and roof that is in board and batten style to meet this criterion.

5. Roof Form.

a. D-MS: Predominate roof form shall be a flat roof with appropriately scaled cornice or stepped parapet top.

b. D-WG: Predominate roof form shall be pitched (4:12 minimum); gabled, hipped, modified pitched roof forms are allowed.

c. D-RR: No restriction on roof form provided the form reflects historic precedence (e.g., depot, ranch/farm buildings, silos/granary tower, historic main street, etc.) and is not an artificial or applied roof form.

- Property is zoned DD-RR and has no restriction to roof form.

J. Materials and Color.

City approval of proposed exterior materials and colors is required for all exterior remodels and alterations regardless of whether site review is required. The Planning Commission may approve adjustments to the following standards pursuant to Section 17.20.070.

1.Exterior Cladding.

a. D-MS: Except as permitted under subsection 'd', below, exterior cladding on new buildings shall predominately consist of durable reddish brick blends, as generally illustrated in Section 17.30.070; brick veneer is also permitted, except as otherwise required for designated historic buildings.

b. D-WG: Except as permitted under subsection 'd', below, exterior cladding of buildings shall predominately consist of wood or fiber cement (lap, panel, board and batten, shingle or similar siding), stucco, stone, textured concrete, split-face concrete block, and/or similar masonry. Corrugated metal may be used as a secondary material only; vinyl siding and faux/cultured stone are not permitted.

c. D-RR: There is no restriction on exterior cladding, provided the design reflects historic precedence of the sub-district (e.g., wood lap, panel, board and batten, shingle, stucco, stone, split-face concrete block, corrugated or sheet metal, etc.) and does not include vinyl siding.

- *Zoned DD-RR and has no restrictions.*

d. Building Additions: Building additions shall conform to the above standards or contain cladding similar to the original cladding of the structure. For purposes of this subsection, "original" means the cladding (material and detailing) used when the building was first constructed.

- No building additions are proposed or possible due to the property line to the North and West, existing structure to South and the East side will be where the vehicles enter and exit the building.

e. Secondary Cladding. Any material permitted above may be used as secondary exterior cladding in the respective sub-district. Metals such as copper, steel, iron, bronze and similar appearance metals may be used as trims or accents (e.g., flashing, wainscoting, weather protection features, ornamentation, etc.) when non-reflective and consistent with historic precedence. In addition, rough-hewn wood, and timbers may be used as accents in the D-WG and D-RR sub-districts.

- The City Hall project will include timbers and will meet this criterion. The vehicle building is utilitarian and simple. Metal choices on this building will be non-reflective.

2. Four Sides of Building. New buildings shall be designed so that all four sides contain complementary exteriors. Changes in material, texture or detailing (e.g., use of two or more different types of material) shall break up otherwise blank walls and define a building's base, middle and top. Side and rear elevations that do not face a street, plaza, patio, or pedestrian access way need not have two or more types of material where changes in texture or detailing break up the wall, consistent with the overall composition of the building.

- The front of this building is broken up by 4 large roll up doors. The doors will complement both the siding and roof colors. The rear of the building is blocked by green arborvitae trees, and the South side has a shop building four feet away. These sides do not need two types of materials. The North side of the new building will face an alley/driveway and also does not need two types of materials to break up the blank walls.

3. Roofs.

a. Where pitched roofs are proposed, roof surfaces shall be wood, slate, cement tile, asphalt shingles, flat metal, or standing rib seam sheet metal.

- This building is proposed rib seam sheet metal and meets this criterion.

b. Metal roofing shall have a non-glare (e.g., matte finish).

- New building metal roofing will be a matte finish and not glare meeting this criterion.

c. Where flat roofs are proposed, cornices and parapets shall incorporate materials that are consistent with historic precedence, as applicable, or otherwise consistent with the overall composition of the building.

- Flat roof not proposed- not applicable.

d. Roofs must be non-reflective and light in color (e.g., light gray or ash, brown, or other earth-tone), and not clash with exterior cladding.

- Roof will be light in color and non-reflective and will match exterior cladding. It will also abide by the downtown design standards for color palette.

4. Windows and Doors. All windows and doors must have wood or vinyl-coated wood trim, or masonry trim and sills. See also, Pedestrian Shelter requirements under subsection 17.30.060(K).

- The new building plans call for metal-coated wood trim. City is asking for a modification to change from vinyl to metal.

5. Substitute Materials. The Planning Commission may approve an adjustment allowing substitute materials that are equal in appearance and durability to those listed above, provided such materials are historically appropriate. The applicant will be required to provide specifications from the manufacturer.

- Plans from the manufacturer are included in the packet for review.

6. Color. Paint colors shall be consistent with the historic color palette as generally illustrated in Section 17.30.070 and on file at Carlton City Hall. Painting schemes shall be simple and coordinated over the entire building to establish a sense of overall composition. Reflective, luminescent, sparkling, and "day-glow" colors and finishes, and clashing paint colors or patterns are prohibited. Metals shall be matte finish, earth-tone color, or burnished/non-reflective (e.g., metal that has a non-reflective finish is allowed in the RR sub-district).

- The metal building colors will be simple, non-reflective and match the historical color palette on file at City Hall.

K. Pedestrian Shelters.

Within the D-MS sub-district, awnings, canopies, recesses or similar pedestrian shelters shall be provided along at least sixty (60) percent of a building's ground floor elevation(s) where the building abuts a sidewalk or civic space (e.g., plaza). Within the D-WG and D-RR sub-districts, the minimum pedestrian shelter requirement is forty (40) percent.

- This building does not have public access for security and safety reasons. Police Officers have community safety equipment as well as computers with access to records. The public will be discouraged from waiting or standing in this area. Also, because of the placement on the property, the only side that could allow for a shelter would be the front facing the street. This side of the building only has roll up doors and doesn't have room for an additional door. An exemption to pedestrian shelters is requested for these reasons.

1. Pedestrian shelters used to meet the above standard shall extend at least five (5) feet over the pedestrian area, be proportionate to the building in its dimensions, and not obscure the building's architectural details.

2. *Pedestrian shelters shall align with one another to the extent practical.*
3. *Shelters shall not conflict with mezzanine or transom windows.*
4. *Colored canvas (not plastic) awnings and metal or plexi-glass canopies, when consistent with historical styles, are allowed.*

Exceptions: Pedestrian shelters are not required where historical precedence dictates otherwise. In addition, the Planning Commission may reduce the minimum shelter depth upon finding that existing right-of-way, easements, or building code requirements preclude a standard shelter.

- Exemption requested- see above.

L. Civic Space and Pedestrian Amenities.

1. Purpose. The City encourages the provision of civic space in new development through regulatory incentives. Civic space such as plazas, courtyards, patios, and expanded sidewalks/outdoor seating areas should be provided along street frontages and where gaps between buildings occur. Civic spaces can make the downtown more attractive and inviting while providing informal gathering places for rest and socialization.

2. Applicability. The following applies to site review proposals involving: (a) a height bonus pursuant to subsection 17.30.060(H); (b) an adjustment to a code standard under site review; or (c) where the applicant voluntarily provides civic space.

3. Civic Space Standards. At least three (3) percent, or not less than three hundred (300) square feet, of the site should be designated and improved as civic space (plaza, landscaped courtyard, sidewalk extension, or similar space). Such areas should be accessible to the general public, with the highest priority locations being those areas with the highest pedestrian activity. Civic spaces should be connected to a public right-of-way by a sidewalk or pedestrian access way. All civic spaces shall have dimensions that allow for reasonable pedestrian access. For example, a small site may provide a 4-foot wide strip adjoining and adding on to the sidewalk for a small café seating area, whereas a larger site at a street corner may provide a plaza adjacent to a building entrance. Civic spaces shall include pedestrian amenities, per subsection 4, below. See also, Chapter 17.84 Site and Landscaping Design.

- The property where the new shop will be located has the highest pedestrian traffic along East Main street. The new vehicle building will face and have access off of 1st street. Civic space not proposed, only a parking lot.

4. Pedestrian Amenities Standards. Where street frontage improvements are required to comply with the Transportation System Plan, or where or civic space is required under subsection 17.30.60(L), street frontages shall be improved with pedestrian amenities such as benches, public art, pedestrian-scale lighting, shade structures, way finding signs, or similar pedestrian facilities in an amount equal to or greater than one-half of one percent (0.5%) of the estimated construction cost of the proposed building(s), subject to review and approval by the Planning Commission. Where a civic space adjoins a building entrance, the cost of providing a weather protection canopy, awning, arcade, overhanging eave, arbor, portico, or similar feature, consistent subsection 17.30.060(K), may be credited toward the one-half of one percent (0.5%) requirement. Pedestrian amenities such as seating, planters, public art and pedestrian lighting (e.g., street lamps or pathway bollard lights) at street corners or paved mid-block pedestrian access ways between buildings may also be counted toward the one-half of one percent

(0.5%) requirement. The cost of a proposed public-private parking facility may be subtracted from building costs used in the assessment of civic space improvements. A licensed architect, landscape architect, or other qualified professional, shall prepare cost estimates for civic space improvements, which shall be subject to review and approval by the Planning Commission.

- The new vehicle building will have the same amount of traffic as the current building. It will not create more. The new parking lot to be installed in front of the vehicle building which will be public parking will increase traffic to 1st street along the property frontage. CDC 17.64.030(E)(2) states, *“For infill development that does not include partitioning or subdivision, construction of sidewalks, including curb and gutter where necessary, along all property frontages shall be the minimum requirement of development. A three-quarter street improvement shall be required if the city engineer determines that the existing streets are not in condition to handle projected traffic loads.”* Due to the increase in traffic and the current state of 1st street, the City is proposing to completion of a sidewalk, curb and gutter on 1st street where there is not currently any pedestrian. If design and space allows, a sitting bench could be added if plants are removed, it can fit. Otherwise, pedestrian amenity additions will be a part of the larger City Hall project.

M. Signs.

In addition to complying with requirements of Chapter 17.80 Signs, conformance to following standards is required in the Downtown district:

1. *Building designs shall incorporate a sign band or otherwise provide for blade signs, awning signs, marquees, or other compatible sign types.*
2. *Pole signs are prohibited.*
3. *Monument signs shall not exceed six (6) feet in height and forty-eight (48) square feet of sign face for each side of a two-sided sign. A minimum of fifty (50) feet of street frontage is required for one monument sign.*
4. *On multitenant buildings, signs shall be designed to accommodate multiple tenants.*
 - No signage is proposed with this project. Not applicable.

N. Landscaping.

In addition to complying with requirements of Chapter 17.84 Site and Landscaping Design, conformance to following standards is required in the Downtown district:

1. *All Downtown Sub-districts: Landscape designs must be compatible with the downtown, where buildings are generally placed closer together than in other districts. Designs take into consideration exposure to sun and wind, opportunities for pedestrian safety and comfort (e.g., buffering vehicle areas, summer shade, etc.), maintenance requirements, and downtown beautification objectives. These factors are to be balanced so that the resulting design is functional, attractive, and cost-effective. For example, landscaping must define pedestrian pathways, buffer parking lots from outdoor seating areas, break up large blank walls, and add color and interest to streetscapes, parking lots, and plaza areas.*
2. *D-MS: Zero (0) percent, minimum, except for required street trees (twenty-five (25) square feet per tree planting area), required civic space, and any required screening, as applicable, subject to site review.*
 - Not applicable due to zone.
3. *D-WG and D-RR: Ten (10) percent, minimum; may include required street trees (twenty-five (25) square feet per tree), planter beds, plant containers or window boxes, arbors, trellises, and climbing*

vines on garden walls and fences, as applicable. Where landscape structures such as arbors are proposed, the Planning Commission may count the total surface area to be covered by plants within two (2) years of planting.

- Some plantings may be added along the property lines. This includes 2 trees and a variety of plants and shrubs. More plantings will be added with the larger City Hall project.

4. Maintenance: All landscape and civic space areas shall be maintained and, as necessary, replaced by the property owner to ensure plant survival and upkeep of street furnishings, paving, and other built features. The use of water-conserving features, such small parking lot perimeter swales, and rain gardens fed by cisterns or roof drains is encouraged.

- When landscape is created the City will do all maintenance and upkeep of these areas. Water conservation features will be completed in the larger project where there is more room and options to add meaningful additions. The vehicle building project is hampered by location on lot and options because of this.

O. Mechanical Equipment.

1. Building Walls. Mechanical equipment shall not be mounted on any building where it will be visible within one hundred (100) feet of Main Street. When mechanical equipment, such as utility vaults, air compressors, generators, antennae, satellite dishes, or similar equipment, must be installed on a rooftop or adjacent to a building wall, it shall be screened from view. Where such equipment is installed on a side or rear building elevation and is adjacent to a plaza, pathway, or other public space, it shall be screened in accordance with Chapter 17.84. Standpipes, meters, vaults and similar incidental equipment need not be screened but shall not be placed on a front elevation when other practical alternatives exist; such equipment shall be placed low on a side or rear elevation to the extent practical.

- There is no mechanical equipment involved in this project- Criterion not applicable.

2. Rooftops. Except as provided below, rooftop mechanical units shall not be visible from the street or any alley, pedestrian access way or civic space. Such units should be screened behind a parapet wall or painted with muted, earth-tone colors that make them visually subordinate to their backgrounds. Exception: Equipment for small-scale renewable energy (e.g., mini-wind turbines, solar panels, and similar features) is allowed subject to site review; the Planning Commission may exempt such a facility from the screening standard upon finding that the screening would interfere with its operation, and the facility does not adversely impact any buildings of local historic significance.

- No mechanical equipment is proposed in this project- not applicable.

3. Ground-Mounted Mechanical Equipment. Ground-mounted equipment (e.g., generators and air compressors) shall be limited to side or rear yards and screened in accordance with Chapter 17.84. The City may require additional setbacks and/or noise attenuating equipment to promote compatibility with adjacent uses.

- There is not any ground mounted equipment in this proposal. If the case arises when a generator is needed, it will be placed in the 4 space between the two shop buildings. This will be a safe spot that is screened and will buffer generator noise for the neighboring properties.

4. Plazas and Open Spaces. Mechanical equipment and garbage storage areas are not permitted within plazas or other public open spaces, except as approved with a conditional use permit. Where such

facilities are allowed, the Planning Commission may require that such facilities be screened completely from view and set back from a civic space for aesthetic reasons and to minimize odors or noise.

- The garbage storage will not be moved from its current location, which is not near the location of the new or existing vehicle storage building. It is currently located between City Hall and Police Department buildings for staff and Recology access. The garbage is collected weekly and with its current location odors are minimized. The garbage area will also be difficult to see and inaccessible to the public in the parking area.

P. Historic Building Alterations.

In addition to complying with requirements of Chapter 17.104 Historic Sites, conformance to following standards is required historic building alterations in the Downtown district:

1. Restoration, rehabilitation or remodeling projects shall incorporate, whenever possible, original design elements that have been removed, changed, or covered over.

- The current building is not considered historic and on not on any historic registers. The current building was placed around 1960. The new building will have similar elements to the old such as the same footprint as the old and have the same material siding and roof. The doors will be roll up on the new building as opposed to the current sliding doors.

2. Scale, proportion and materials used in alteration or additions to existing structures, such as the size and relationship of new windows, doors, entrances and other building features, shall be visually compatible with the original architecture on buildings of local historical significance.

- See above.

City of Carlton

CITY HALL PARKING LOT

15350 SW Sequoia Parkway, Suite 220
 Portland, OR 97224
 Phone: (503) 684-9097 Fax: (503) 598-0583

www.tetrattech.com



TETRA TECH

PROJECT LOCATION:

191 E Main St
 Carlton, OR

CLIENT INFORMATION:

City of Carlton

Tt PROJECT No.:

135-13914-16001

CLIENT PROJECT No.:

PROJECT DESCRIPTION / NOTES:

ISSUED:

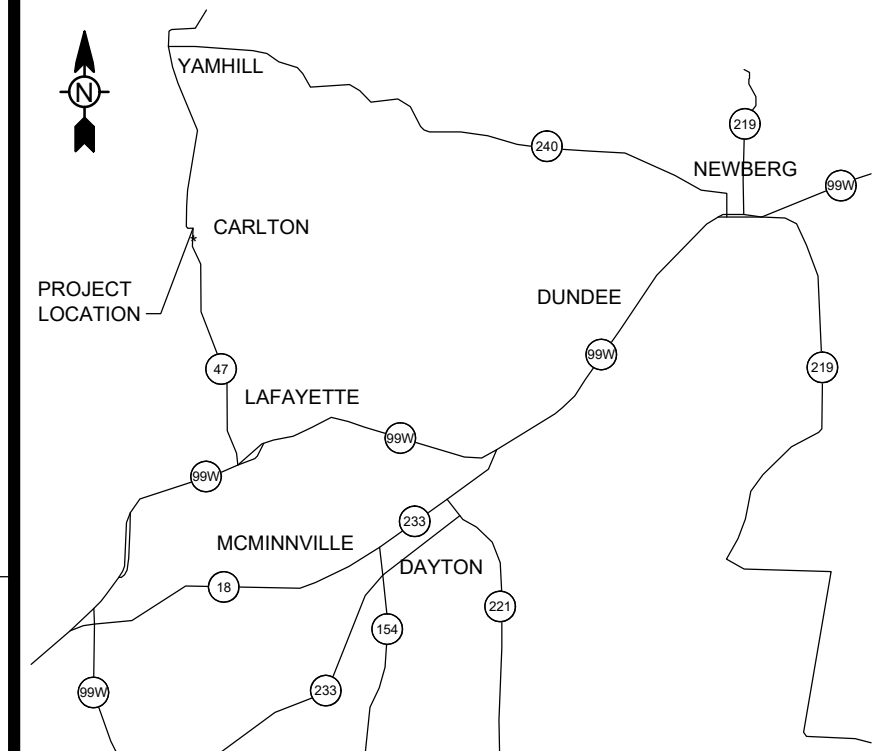
VICINITY MAP:

DRAWING INDEX

<u>SHT #</u>	<u>DESCRIPTION</u>
G01	COVER
G02	SYMBOLS AND ABBREVIATIONS
C01	EXISTING SITE AND DEMOLITION PLAN
C02	LAYOUT PLAN
C03	STRIPING AND FEATURES PLAN
C04	CIVIL DETAILS
C05	CIVIL DETAILS
C06	CIVIL DETAILS
C07	EROSION CONTROL PLAN
C08	EROSION CONTROL DETAILS
E01	LEGEND
E02	POWER PLAN
L01	PLANTING PLAN
L02	IRRIGATION PLAN
L03	LANDSCAPING DETAILS



 **PROJECT SITE MAP**
 NTS



STREET AND SIDEWALK CONSTRUCTION NOTES

1. ALL CONSTRUCTION IS TO BE PERFORMED IN ACCORDANCE WITH CITY STANDARDS, OREGON 2018 APWA/ODOT STANDARD SPECIFICATIONS AND THE OREGON ADMINISTRATIVE RULES (OAR), CHAPTER 333, UNLESS NOTED ON THE PLANS OR SPECIFIED OTHERWISE.
2. THE CONTRACTOR SHALL VERIFY LOCATIONS, ELEVATIONS, TYPES AND SIZES OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTING NEW PIPING. POTHOLES SHALL SUFFICIENTLY PRECEDE LAYING OF PIPE TO ALLOW REQUIRED ELEVATION ADJUSTMENTS TO BE ACCOMPLISHED WITHOUT REWORK. ELEVATION ADJUSTMENTS SHALL BE EXPECTED AND ARE INCIDENTAL TO THE WORK. DEFLECT PIPE AS REQUIRED AND WITHIN (80%) OF MANUFACTURER'S TOLERANCES TO AVOID EXISTING UTILITIES AND COMPLETE TIE-INS.
3. LOCATIONS AND GRADES OF EXISTING UTILITIES ARE BASED UPON INFORMATION PROVIDED BY THE UTILITIES AND ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. OREGON LAW REQUIRES CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER (PHONE NO: 1-800-332-2344) SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR SHALL HAVE UTILITIES LOCATED IN ACCORDANCE WITH ORS 757.541 THROUGH 757.571 PRIOR TO COMMENCING CONSTRUCTION. CONTRACTOR SHALL ALSO FIELD VERIFY DEPTHS OF EXISTING UTILITIES TO IDENTIFY POTENTIAL CONFLICTS AND AS REQUIRED FOR CONNECTIONS TO EXISTING SYSTEMS.
4. ALL EXISTING ROADS, CURBS, SIDEWALKS, LANDSCAPING AND OTHER FACILITIES OUTSIDE THE CONTRACTED WORK WHICH ARE DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO ORIGINAL CONDITION OR BETTER. REPAIR WORK TO BE AT CONTRACTORS EXPENSE.
5. MAINTENANCE OF THE WORK AREA AND APPROACH ROADS IS THE RESPONSIBILITY OF THE CONTRACTOR. THE WORK AREA AND APPROACH ROADS SHALL BE MAINTAINED IN A CLEAN CONDITION, FREE FROM OBSTRUCTIONS AND HAZARDS.
6. UTILITIES OR INTERFERING PORTIONS OF UTILITIES THAT ARE ABANDONED IN PLACE SHALL BE REMOVED BY THE CONTRACTOR TO THE EXTENT NECESSARY TO ACCOMPLISH THE WORK. THE CONTRACTOR SHALL SEAL AND CAP THE REMAINING EXPOSED ENDS OF ABANDONED UTILITIES WITH NON-SHRINK GROUT TO ONE FOOT LENGTH INTO PIPE.
7. ALL EXCAVATIONS SHALL BE BACKFILLED OR COVERED WITH STEEL SHEETS DURING NON-CONSTRUCTION HOURS. STEEL SHEET PLACEMENTS SHALL BE LIMITED TO 36 HOURS IN ANY ONE LOCATION. BEYOND 36 HOURS, EXCAVATIONS SHALL BE BACKFILLED. TEMPORARY OR PERMANENT AC PAVEMENT SHALL BE PLACED ON ALL BACKFILLED EXCAVATIONS. AC PAVEMENT, EITHER TEMPORARY OR PERMANENT, SHALL BE HOT MIX BITUMINOUS ASPHALT.
8. STAGING AND STORAGE OF MATERIALS WITHIN THE PUBLIC RIGHT-OF-WAY IS NOT PERMITTED EXCEPT AS SPECIFICALLY AUTHORIZED.
9. SHOULD WATER LINE VALVES BE REQUIRED TO BE OPERATED, OPERATION SHALL BE BY CITY OF CARLTON PERSONNEL ONLY.
10. ALL EXISTING PAVEMENT MARKINGS DISTURBED OR REMOVED DURING CONSTRUCTION SHALL BE REPLACED. STRIPING MATERIALS SHALL MATCH OR EXCEED EXISTING.


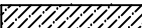
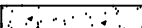
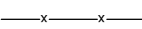
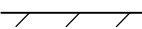

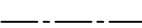
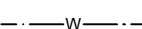
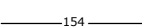













TRAFFIC CONTROL NOTES

1. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH A TRAFFIC CONTROL PLAN SUBMITTED BY THE CONTRACTOR AND APPROVED BY THE CITY OF CARLTON. A COPY OF THE APPROVED PLAN SHALL BE AVAILABLE AT THE WORK SITE AT ALL TIMES. THE CITY RESERVES THE RIGHT TO MODIFY TRAFFIC CONTROL AS MAY BE NECESSARY TO INSURE PUBLIC SAFETY.
2. VEHICULAR AND PEDESTRIAN ACCESS TO PROPERTIES SHALL BE MAINTAINED AT ALL TIMES INCLUDING NORMAL DELIVERY AND MAIL SERVICE, UNLESS WRITTEN APPROVAL OF THE ENGINEER IS PROVIDED.

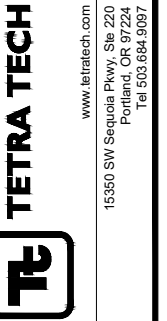
ABBREVIATIONS

AC	ASPHALTIC CONCRETE	MIN	MINIMUM
APPROX	APPROXIMATELY	NO.	NUMBER
BM	BENCHMARK	NTS	NOT TO SCALE
BOC	BOTTOM OF CURB	ODOT	OREGON DEPARTMENT OF
CB	CATCH BASIN		TRANSPORTATION
CL	CENTERLINE	PP	POWER POLE
CONC	CONCRETE	PT	POINT
COORD	COORDINATE(S)	PVC	POLYVINYL CHLORIDE
DET	DETAIL	PVMT	PAVEMENT
DIA	DIAMETER	ROW	RIGHT-OF-WAY
DWG	DRAWING	REQ'D	REQUIRED
DWY	DRIVEWAY	RIM	RIM ELEVATION
E	ELECTRICITY	S/W	SIDEWALK
EL. ELEV	ELEVATION	SD	STORM DRAIN
EOP	EDGE OF PAVEMENT	SHT	SHEET
EXST	EXISTING	SL	SLOPE
FH	FIRE HYDRANT	SPEC	SPECIFICATIONS
FT	FEET	SS	SANITARY SEWER
G	GAS	STD	STANDARD
HORIZ	HORIZONTAL	T, TEL	TELEPHONE
IE	INVERT ELEVATION	TL	TAXLOT
IR	IRON ROD	TOC	TOP OF CURB
LF	LINEAR FEET	TYP	TYPICAL
MAX	MAXIMUM	W	WATER
MATL	MATERIAL	W/	WITH

LEGEND - EXISTING

	AC
	GRAVEL
	CONCRETE
	FENCE
	BUILDING
	PAVEMENT EDGE
	PAVEMENT CENTERLINE
	WATER LINE
	CONTOUR
	STORM PIPE
	OVERHEAD POWER LINE
	CONCRETE
	WM WATER METER
	WV WATER VALVE
	CB CATCH BASIN
	POWER POLE
	STORM DRAIN
	MAILBOX
	GUY WIRE
	TB TELEPHONE BOX
	TREE
	FIRE HYDRANT

8/9/2019 2:24:53 PM - P:\13914\135-139\14-16001\CAD\SHEETFILES\CITY HALL PARKING LOT\G02 SYMBOLS AND ABBREVIATIONS.DWG - MARTINEZ, LESLEY



MARK	DATE	DESCRIPTION	BY

CITY OF CARLTON
City Hall Parking Lot




SYMBOLS AND ABBREVIATIONS

PROJ:	135-13914-16001
DESN:	GJM
DRWN:	LEM
CHKD:	GJM

G02

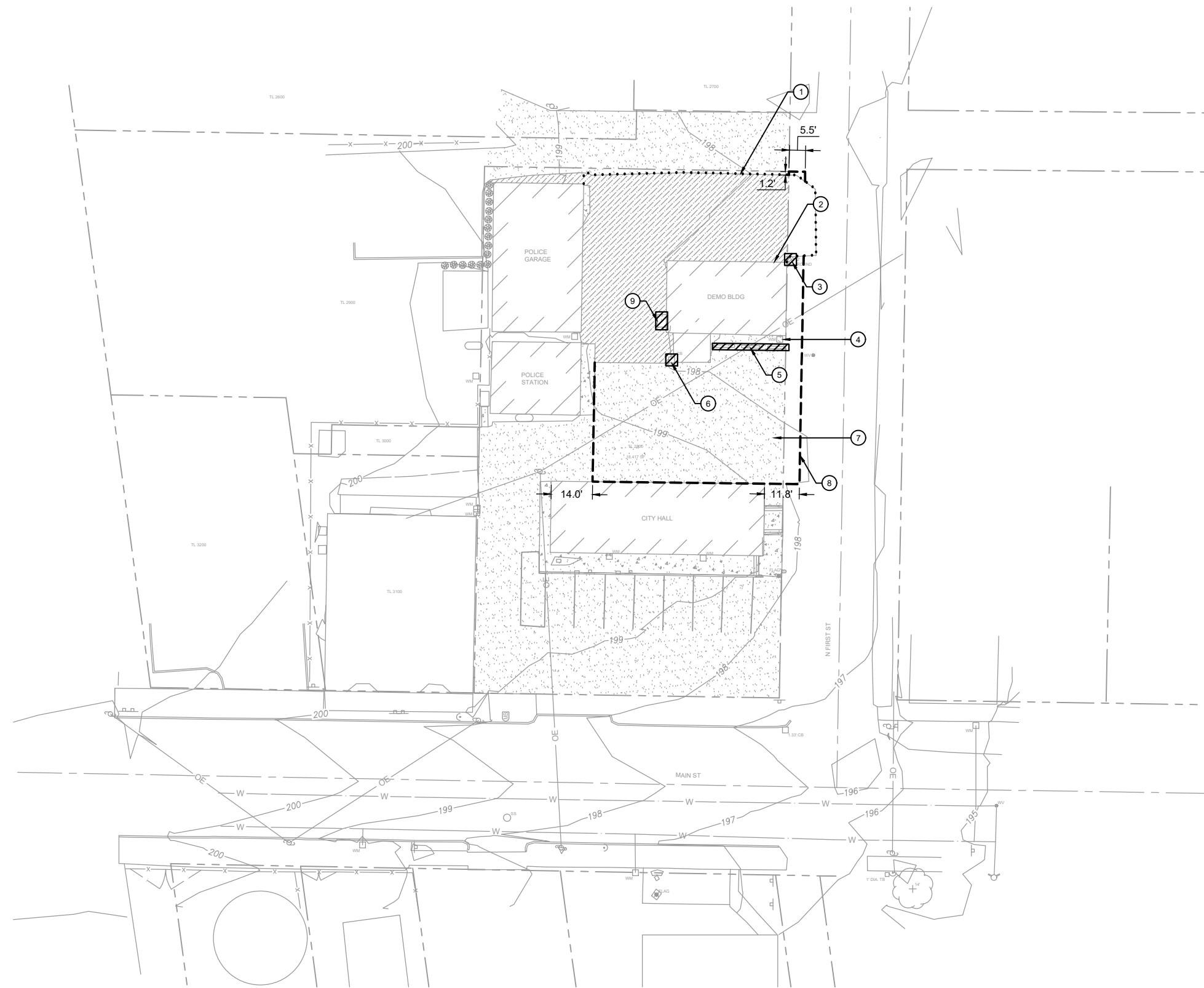
Copyright: Tetra Tech

LEGEND - PROPOSED

-  DEMOLITION
-  SAWCUT
-  GRAVEL EXCAVATION BOUNDARY

DEMOLITION NOTES

- ① EXCAVATE EXIST GRAVEL TO ACCOMMODATE PAVEMENT SECTION; APPROX. 4,000 SQFT AREA
- ② BUILDING DEMOLISHED PRIOR TO PROJECT
- ③ REMOVE EXIST CB; PREVENT SEDIMENT FROM ENTERING EXIST STORM PIPE AND INSTALL TEMPORARY CAP PRIOR TO RECONNECTION
- ④ RELOCATE EXIST WM ACROSS STREET
- ⑤ REMOVE EXIST CONCRETE CURB, TYP (3)
- ⑥ REMOVE EXIST CB; PREVENT SEDIMENT FROM ENTERING EXIST STORM PIPE AND INSTALL PERMANENT CAP
- ⑦ REMOVE EXIST AC, APPROX. 3,005 SQFT AREA
- ⑧ SAWCUT EXIST AC, APPROX. 200 FT
- ⑨ REMOVE EXIST CONCRETE, APPROX. 13 SQFT AREA



1
EXISTING SITE AND
DEMOLITION PLAN
SCALE: 1" = 20'

8/9/2019 9:37:13 AM - P:\13914\135-13914-16001\CAD\SHEET\CITY HALL PARKING LOT\C01 EXISTING AND DEMO PLAN.DWG - MARTINEZ, LESLEY

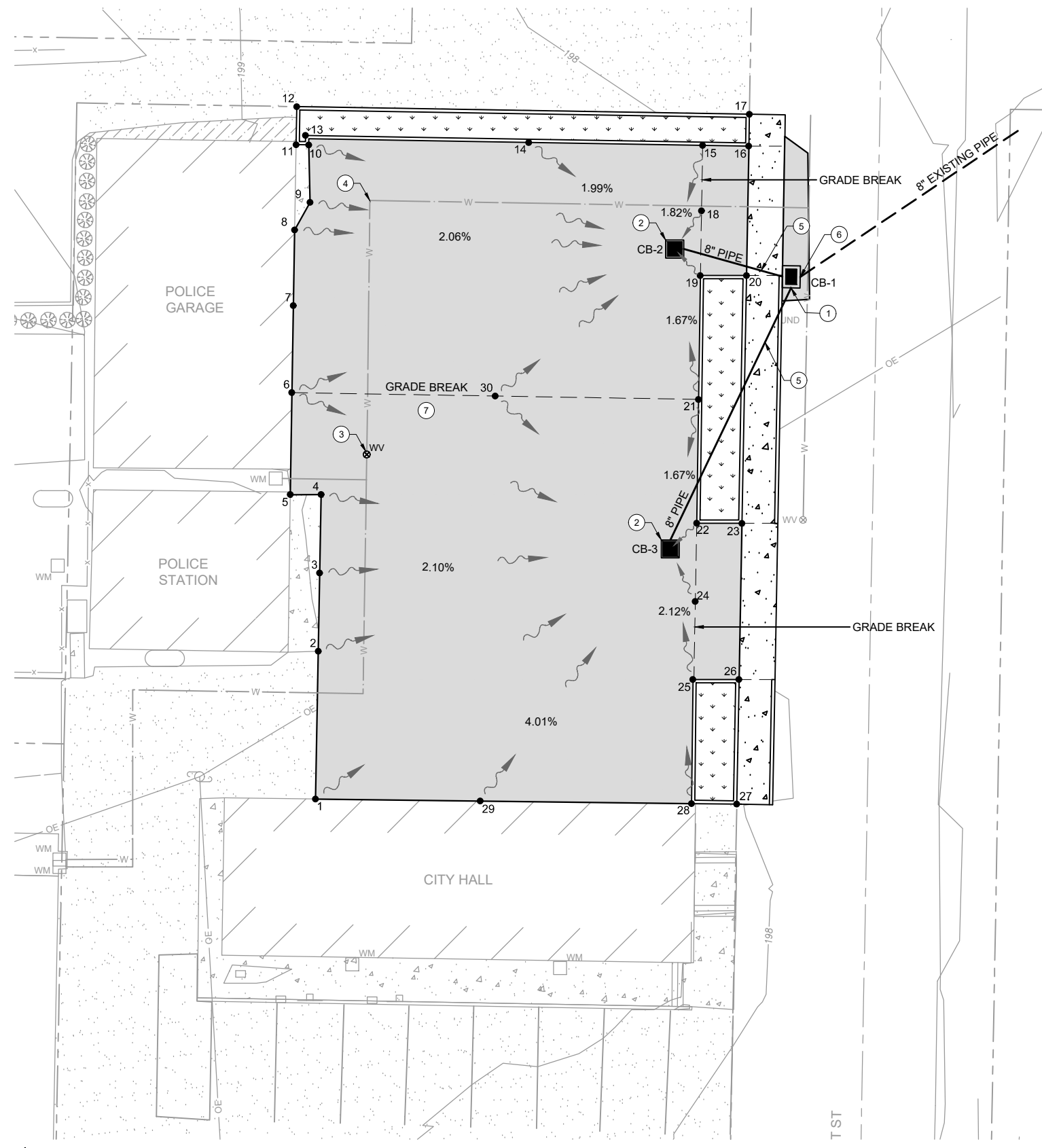
MARK	DATE	DESCRIPTION

CITY OF CARLTON
City Hall Parking Lot
**EXISTING SITE AND
DEMOLITION PLAN**

PROJ: 135-13914-16001
DESN: GJM
DRWN: LEM
CHKD: GJM

C01

TETRA TECH
www.tetra-tech.com
15350 SW Sequoia Pkwy, Ste 220
Portland, OR 97224
Tel: 503.684.9097



LEGEND - PROPOSED

- SEWER LINE
- CATCH BASIN
- WATER VALVE
- DRAINAGE DIRECTION

TABLE 1: STORM DRAIN STRUCTURE DATA

ID	DESC.	RIM ELEVATION	X	Y	INVERT ELEVATION
CB-1	TYPE G-1	197.20	20762.94	20438.71	IE IN (W): 195.15 IE IN (S): 195.15 IE OUT: 195.10
CB-2	PRECAST	197.75	20745.05	20443.87	IE OUT: 195.25
CB-3	PRECAST	197.75	20744.37	20397.88	IE OUT: 195.50

CONSTRUCTION NOTES

1. INSTALL CB PER $\frac{2}{C05}$ AND GRATE PER $\frac{3}{C05}$
2. INSTALL CB PER $\frac{1}{C05}$
3. INSTALL WV PER $\frac{1}{C06}$
4. APPROX. LAYOUT OF EXISTING WATER SERVICE LINE; (POTHOLE TO FIELD VERIFY)
5. INSTALL APPROX. 60 LF 8" PVC C900 DR18
6. RECONNECT TO EXIST 8" PIPE W/CPG AND PIPE STUD AS REQ'D
7. SLOPE OF AC WILL BE ADJUSTED DURING CONSTRUCTION BUT SHALL REMAIN BETWEEN 0.50% AND 4.01%; NO LOW SPOTS SHALL BE CONSTRUCTED BEYOND THE CB; % GIVEN AS GUIDANCE, SEE ELEVATIONS IN TABLE 2 AND ADJUST SPECIFIC % SLOPE ACCORDINGLY

TABLE 2: POINT DATA

POINT #	ELEV. (FT)	X	Y
1	199.75	20689.98	20359.53
2	199.04	20690.39	20382.18
3	198.88	20690.62	20394.19
4	198.89	20690.84	20406.21
5	199.12	20686.12	20406.18
6	198.87	20686.36	20421.85
7	198.87	20686.57	20435.21
8	198.95	20686.77	20446.79
9	198.68	20689.14	20451.03
10 (BOC)	198.55	20688.94	20459.84
11 (TOC)	199.13	20687.00	20459.87
12 (TOC)	199.27	20687.09	20465.71
13 (TOC)	199.13	20688.45	20461.29
14 (BOC)	198.30	20722.68	20460.22
15 (BOC)	198.05	20749.36	20459.77
16 (TOC)	198.42	20756.43	20459.65
17 (TOC)	198.36	20756.51	20464.55
18	197.93	20749.17	20449.79
19 (BOC)	197.79	20748.99	20439.81
20 (TOC)	197.81	20756.06	20439.81
21 (BOC)	198.14	20748.71	20420.81
22 (BOC)	197.80	20748.43	20401.81
23 (TOC)	198.06	20755.36	20401.81
24	198.01	20748.21	20389.83
25 (BOC)	198.18	20747.99	20377.85
26 (TOC)	198.55	20754.92	20377.84
27 (TOC)	198.55	20754.57	20358.74
28 (BOC)	198.82	20747.63	20358.83
29	199.69	20715.23	20359.22
30	198.51	20717.53	20421.33

8/9/2019 2:28:37 PM - P:\13914\135-13914-16001\CAD\SSHEE\CITY HALL PARKING LOT\C02 LAYOUT PLAN.DWG - MARTINEZ, LESLEY

1 LAYOUT PLAN
SCALE: 1" = 10'

BY	DATE	DESCRIPTION	MARK

CITY OF CARLTON
City Hall Parking Lot
LAYOUT PLAN

PROJ: 135-13914-16001
 DESN: GJM
 DRWN: LEM
 CHKD: GJM

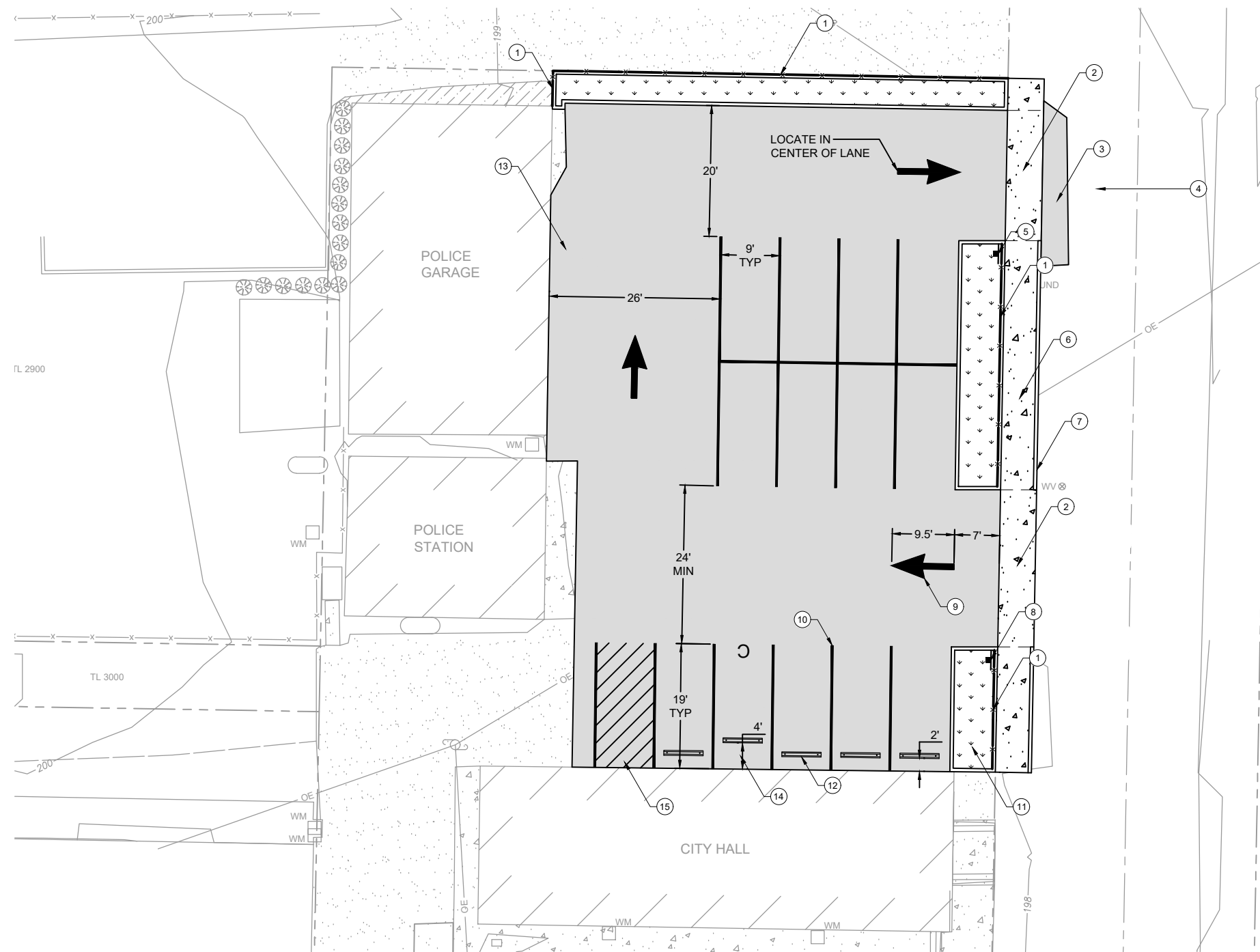
C02

Bar Measures 1 inch, otherwise drawing not to scale

Copyright: Tetra Tech



8/9/2019 2:19:27 PM - P:\13914\135-13914-16001\CAD\SHETS\CITY HALL PARKING LOT\03 STRIPING AND FEATURES PLAN.DWG - MARTINEZ, LESLEY

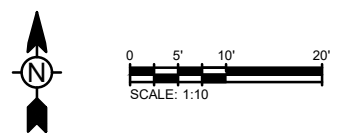


LEGEND - PROPOSED

- LANDSCAPE FEATURE
- AC
- SIDEWALK
- FENCE

CONSTRUCTION NOTES

- ① INSTALL 4 FT WROUGHT IRON PERIMETER FENCE PER ④ C04 SEE C04 FOR FENCE NOTES
- ② INSTALL DRIVEWAY APPROACH PER ⑤ C04
- ③ SAND AND SEAL JOINT AT NEW/EXIST PAVEMENT INTERFACE; INSTALL AC SECTION, APPROX. 95 SQFT AREA, PER ② C04
- ④ EXIST STREET
- ⑤ INSTALL EXIT ONLY SIGN (18" X 18") PER ② C06
- ⑥ INSTALL SWLK SECTION, APPROX. 313 SQFT AREA, PER ⑤ C04
- ⑦ INSTALL TYPE A CURB AND GUTTER, APPROX. 57 LF, PER ① C04
- ⑧ INSTALL ENTRANCE ONLY SIGN (12" X 18") PER ② C06
- ⑨ INSTALL PAVEMENT ARROW
- ⑩ INSTALL 4" WHITE PAVEMENT MARKINGS
- ⑪ SEE DWG L02 FOR PLANTING PLAN, TYP ALL LANDSCAPE FEATURES
- ⑫ INSTALL CONCRETE WHEEL STOP (5) PER ③ C06
- ⑬ INSTALL AC SECTION, APPROX. 7,005 SQFT AREA, PER ③ C04
- ⑭ CONTRACTOR TO VERIFY LOCATION OF EXIST ELECTRICAL BOX AND COORDINATE LOCATION OF COMPACT PARKING STALL; INSTALL WHITE COMPACT PARKING STALL SYMBOL ("C")
- ⑮ CONTRACTOR TO VERIFY LOCATION OF EXIST DOOR AND COORDINATE LOCATION OF NO PARKING ZONE; INSTALL NO-PARKING STRIPING



1 STRIPING AND FEATURES PLAN
SCALE: 1" = 10'

MARK	DATE	DESCRIPTION

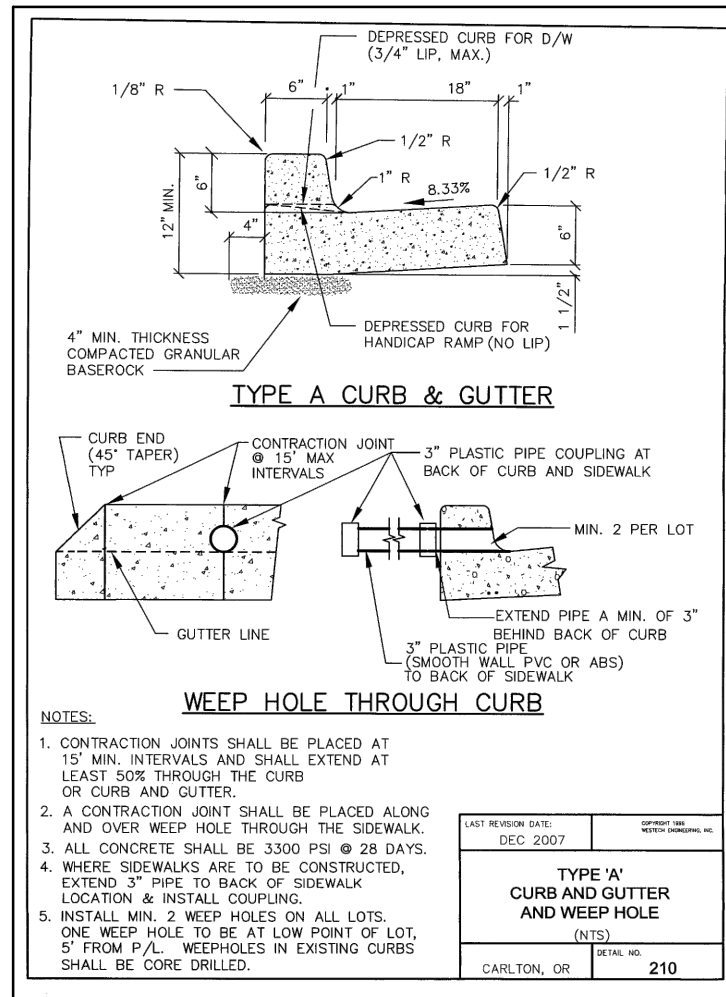
CITY OF CARLTON
City Hall Parking Lot

STRIPING AND FEATURES PLAN

PROJ:	135-13914-16001
DESN:	GJM
DRWN:	LEM
CHKD:	GJM

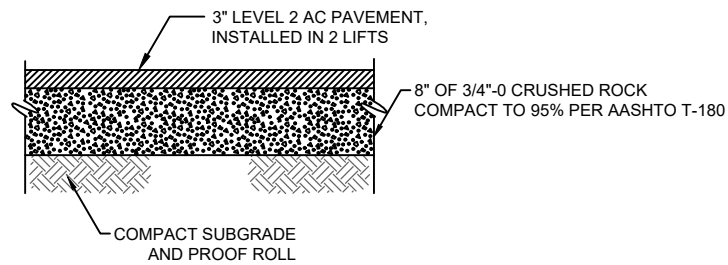
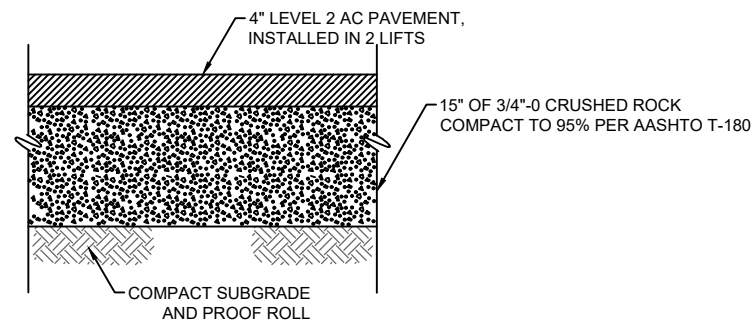
C03

Copyright: Tetra Tech



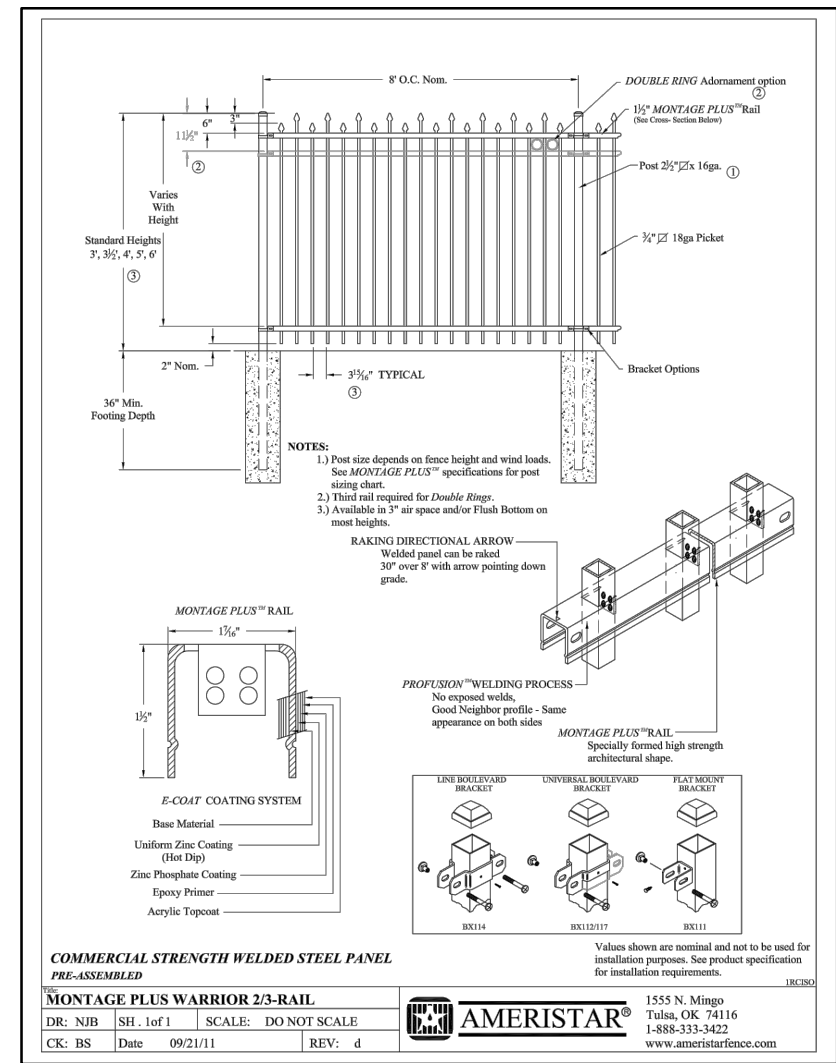
1 CURB, GUTTER, AND WEEP HOLE

SCALE: NONE



3 PARKING LOT SECTION

SCALE: NONE



4 4 FT STEEL PERIMETER FENCE

SCALE: NONE

4 FT PERIMETER FENCE NOTES:

STEEL FENCE:

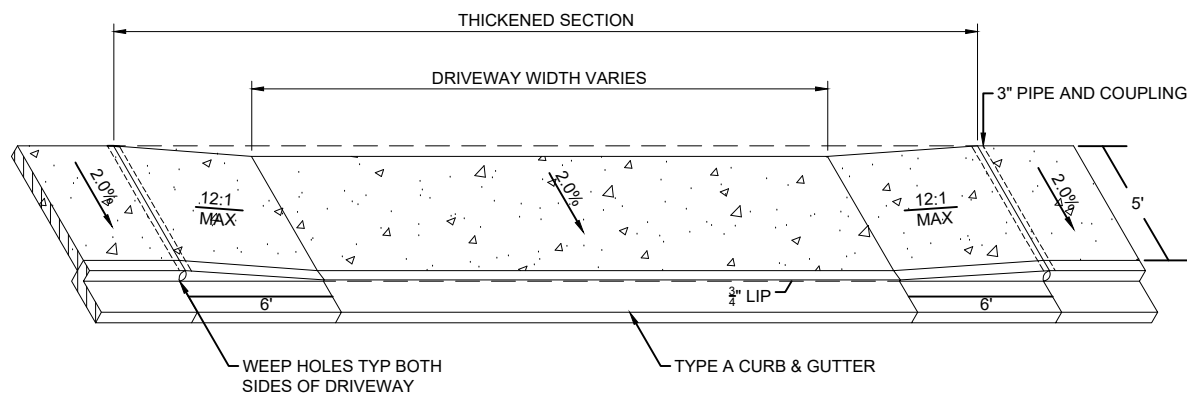
- FENCE PANELS AND POSTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A653/A653M WITH A MINIMUM YIELD STRENGTH OF 45,000 PSI AND A MINIMUM ZINC (HOT-DIP GALVANIZED) COATING WEIGHT OF 0.60 OZ/SF, COATING DESIGNATION G-60.
- PICKETS SHALL BE 3/4" SQUARE X 18 GA TUBING.
- RAILS SHALL BE STEEL CHANNEL, 1.5" X 14 GA. MANUFACTURED BY AMERISTAR OR APPROVED EQUAL.

WROUGHT IRON:

- FABRICATED OF GRAY OR MALLEABLE IRON. GRAY IRON PER ASTM A48/A48M, CLASS 30. MALLEABLE IRON PER ASTM A47/A47M. MANUFACTURED BY AMERISTAR OR APPROVED EQUAL.
- WROUGHT IRON FENCE SHALL HAVE THE APPEARANCE SHOWN IN DETAIL **4 C04**

NOTES:

- CONCRETE DEPTH FOR STANDARD SIDEWALKS SHALL BE 4" MIN.
- DRIVEWAY SECTION INCLUDING SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6" MIN. THICKNESS.
- CONCRETE SHALL BE 3300 PSI @ 28 DAYS.
- PCC APRONS SHALL BE JOINTED TO MATCH SIDEWALK PATTERN.
- SIDEWALKS SHALL BE LOCATED ENTIRELY WITHIN RIGHT-OF-WAY OR SIDEWALK EASEMENTS, INCLUDING SIDEWALKS AT DRIVEWAY APRONS.
- 2.0% CROSS SLOPE IS MEASURED FROM HORIZONTAL. 12:1 SIDEWALK SLOPE IS RELATIVE TO THE RUNNING SLOPE OF THE SIDEWALK.
- MIN. 4" OF 3/4"-0" COMPACTED GRANULAR BASEROCK (TYPICAL UNDER ALL SIDEWALKS AND DRIVEWAYS)



5 SIDEWALK AND DRIVEWAY APPROACH

SCALE: NONE

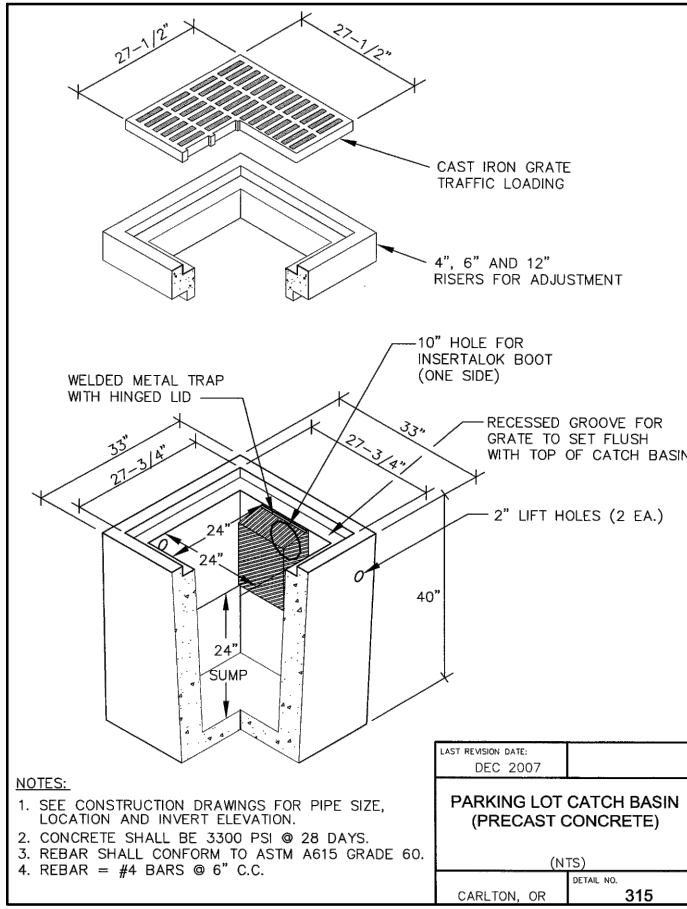
MARK	DATE	DESCRIPTION

CITY OF CARLTON	City Hall Parking Lot	CIVIL DETAILS
Civil Hall Parking Lot		

PROJ:	135-13914-16001
DESN:	GJM
DRWN:	LEM
CHKD:	GJM

C04

8/9/2019 1:57:16 PM - P:\13914\135-13914-16001\CAD\SHHEET\CITY HALL PARKING LOT\04 CIVIL DETAILS.DWG - MARTINEZ, LESLEY



NOTES:

- SEE CONSTRUCTION DRAWINGS FOR PIPE SIZE, LOCATION AND INVERT ELEVATION.
- CONCRETE SHALL BE 3300 PSI @ 28 DAYS.
- REBAR SHALL CONFORM TO ASTM A615 GRADE 60.
- REBAR = #4 BARS @ 6" C.C.

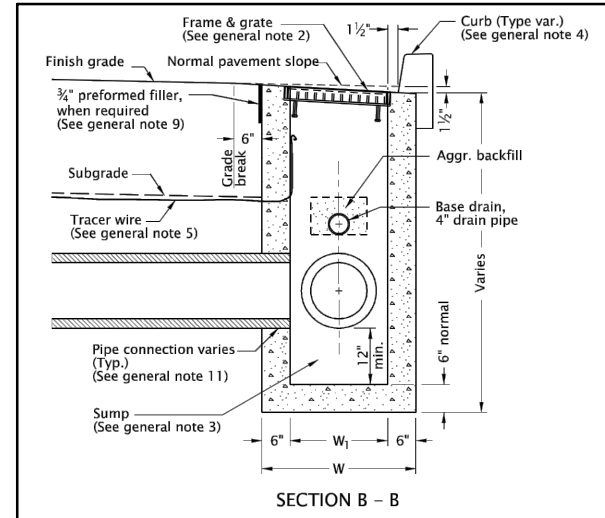
LAST REVISION DATE: DEC 2007

PARKING LOT CATCH BASIN (PRECAST CONCRETE)

(NTS)

CARLTON, OR DETAIL NO. 315

1 PRECAST CATCH BASIN
SCALE: NONE

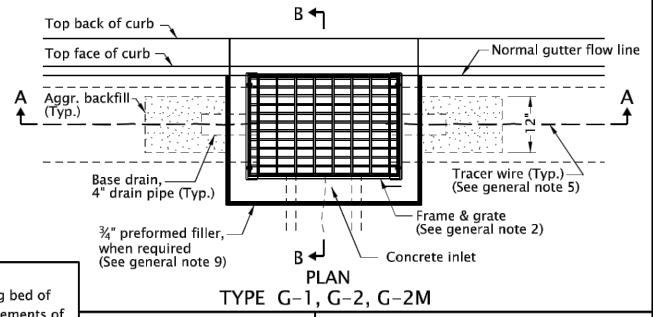
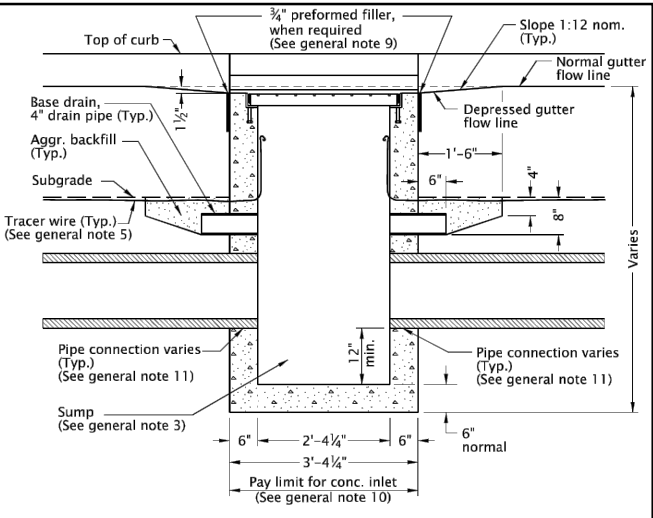


INLET TYPE	W	W ₁
G-1	2'-8 1/2"	1'-8 1/2"
G-2, G-2M, G-2MA	3'-3 3/8"	2'-3 3/8"

GENERAL NOTES FOR ALL DETAILS:

- Where precast inlets are used as an alternate to cast-in-place inlets, a 4" compacted leveling bed of sand or 3/4"-0 crushed aggregate shall be provided. All precast inlets shall conform to requirements of ASTM C913.
- Graphics show G-1 inlet with Type 2 grate. See Table A for inlet dimensions. Type 1 grate allowed only in locations not subject to bicycle or pedestrian use. For frame and grate details, see Std. Dwg. RD365.
- Provide sump only where shown on plans, and allowed by jurisdiction. See Detail A for inlet without sump.
- For curb details, see Std. Dwgs. RD700 & RD701.
- See Std. Dwg. RD336 for tracer wire details, or approved alternate.
- Max. pipe diameter varies with pipe material.
- Location, elevation, diameter, slope, and number of pipe(s) varies, see project plans.
- All concrete shall be commercial grade concrete.
- 3/4" preformed filler (in concrete pavement or gutter only) to extend through thickness of concrete.
- See Std. Dwg. RD363 for gutter transition section, when curb and gutter are required.
- See Std. Dwg. RD339 for pipe to structure connections.

2 TYPE G-1 CONCRETE INLET
SCALE: NONE



CALC. BOOK NO. N/A

BASELINE REPORT DATE

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications

OREGON STANDARD DRAWINGS

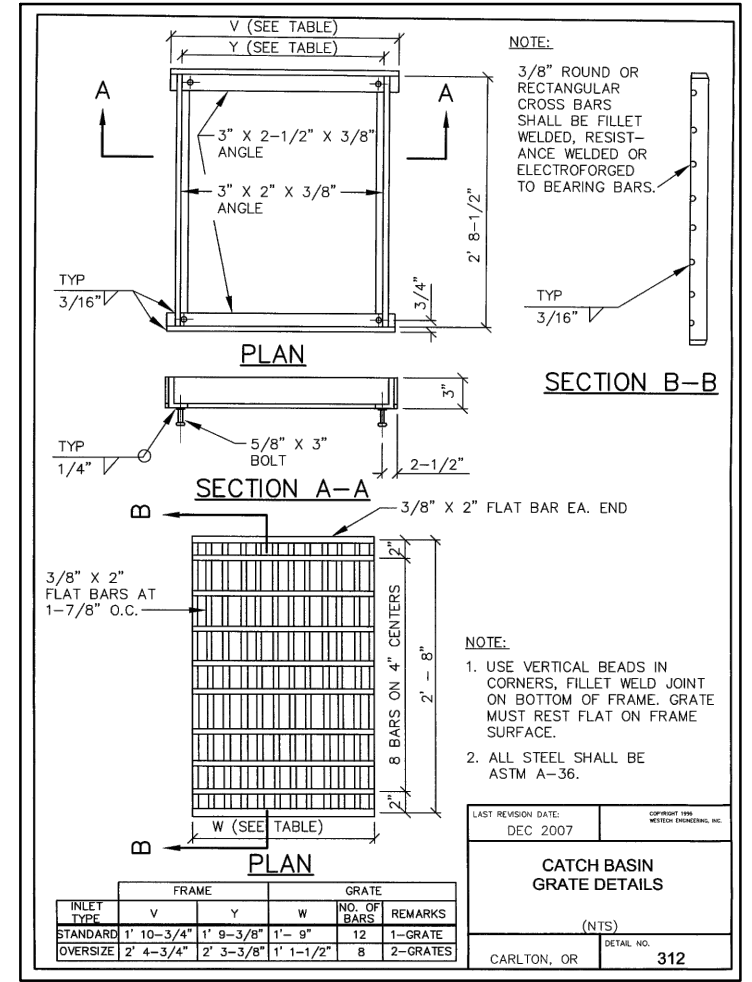
CONCRETE INLETS
TYPE G-1, G-2, G-2M, & G-2MA

DATE 2018

REVISION DESCRIPTION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

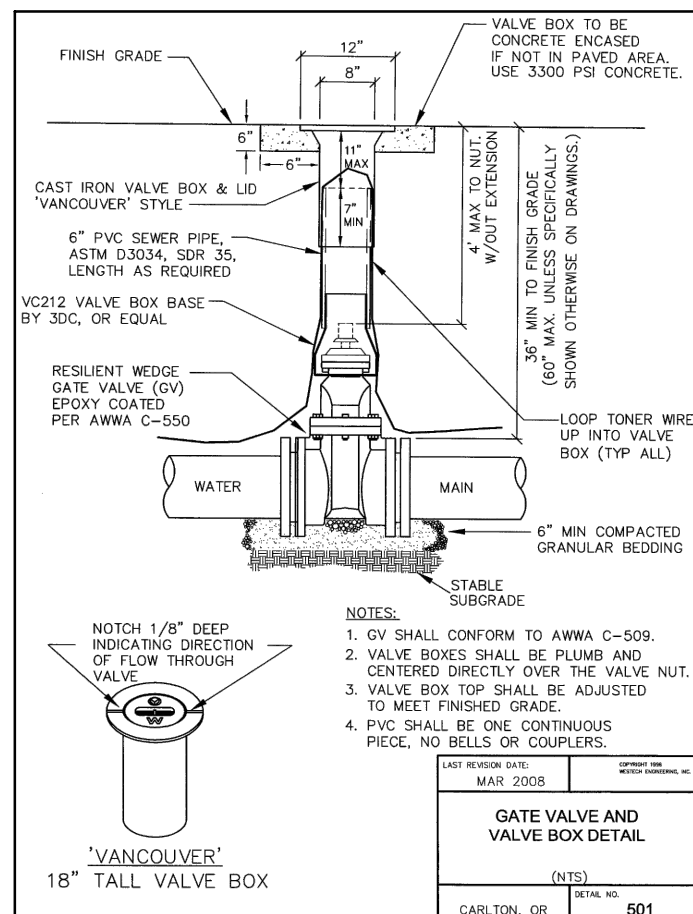
Effective Date: June 1, 2019 - November 30, 2019 RD364



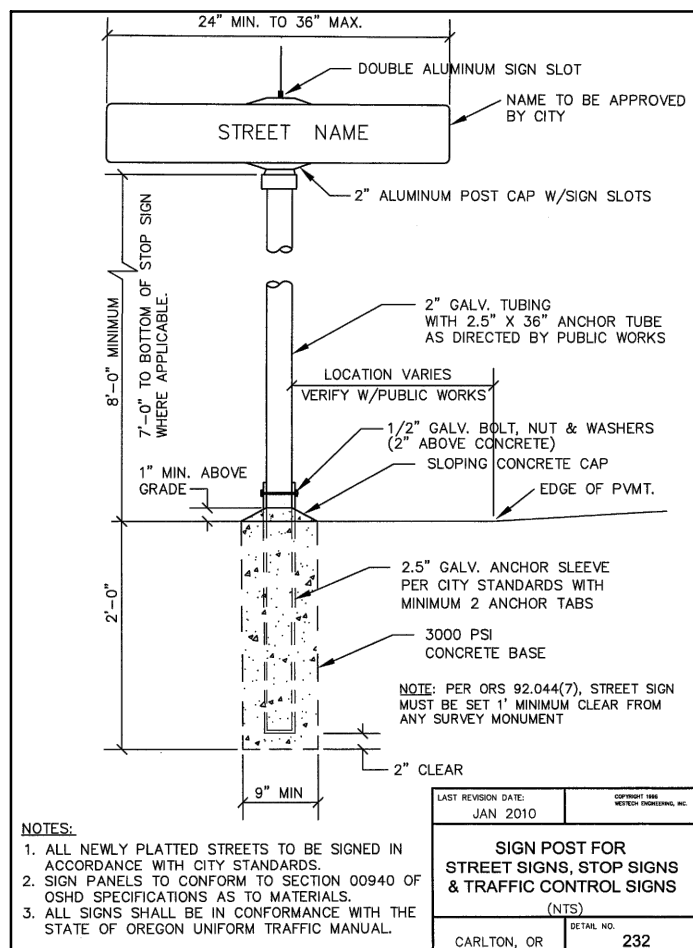
3 TYPE G-1 GRATE
SCALE: NONE

BY	DATE	DESCRIPTION

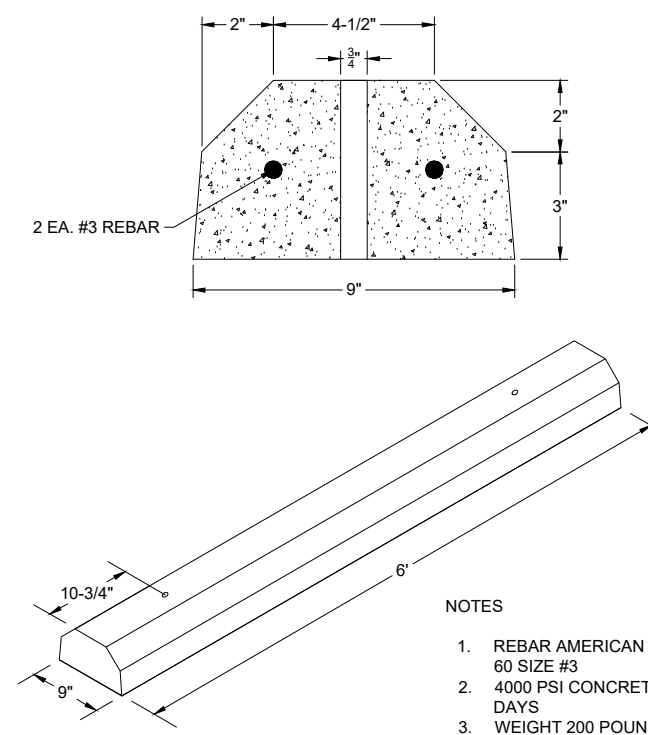
8/5/2019 1:18:41 PM - P:\13914\135-13914-16001\CAD\SHEET\FILES\CITY HALL PARKING LOT\C05 CIVIL DETAILS.DWG - MARTINEZ, LESLEY



1 GATE VALVE AND VALVE BOX
SCALE: NONE



2 SIGN POST FOR TRAFFIC CONTROL
SCALE: NONE



3 CONCRETE WHEEL STOP
SCALE: NONE

8/5/2019 1:20:13 PM - P:\13914\135-13914-16001\CAD\SHEETFILES\CITY HALL PARKING LOT\C06 CIVIL DETAILS.DWG - MARTINEZ, LESLEY

TETRA TECH
www.tetra-tech.com
15550 SW Sequoia Pkwy, Ste 220
Portland, OR 97227
Tel: 503.684.9097

MARK	DATE	DESCRIPTION	BY

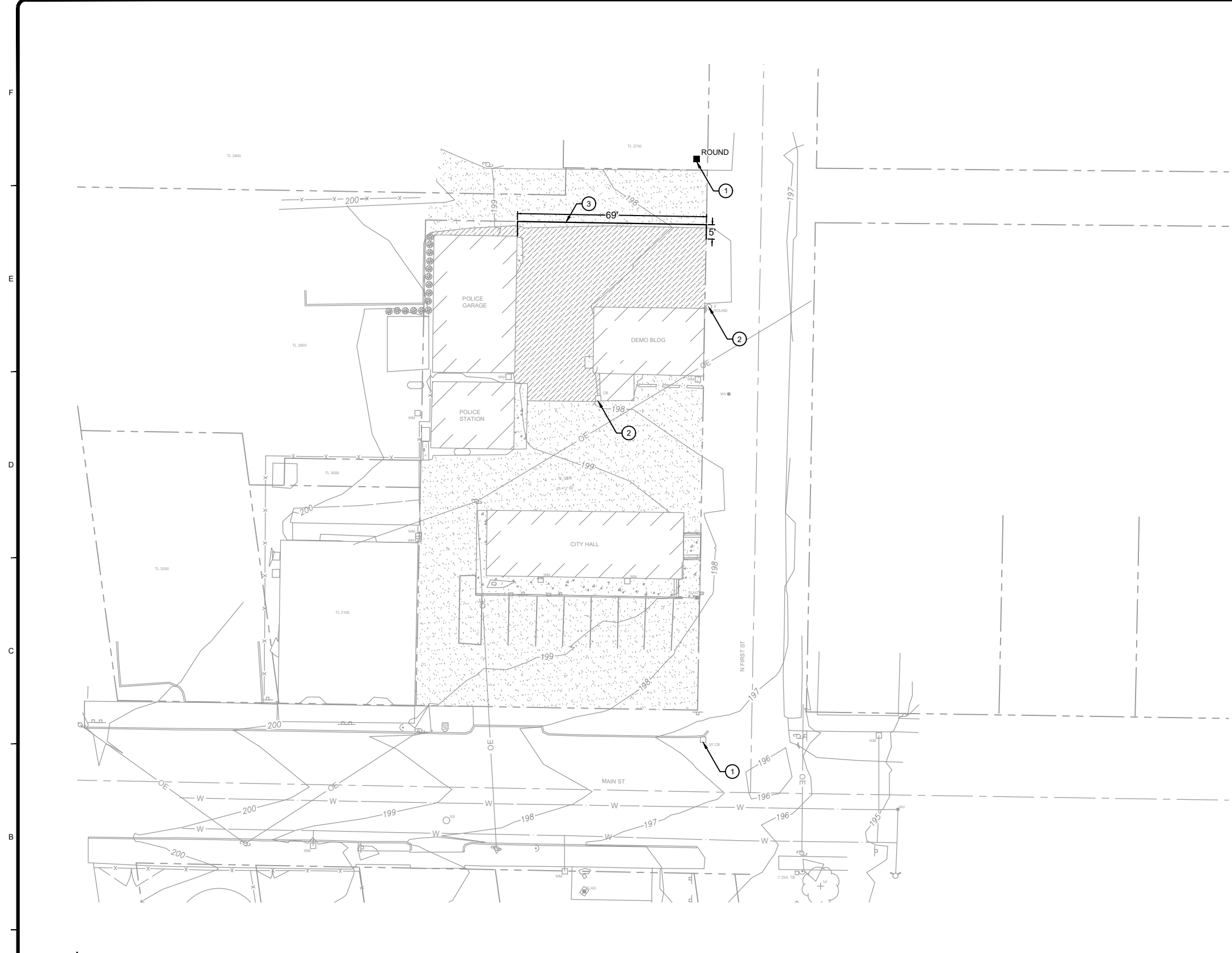
CITY OF CARLTON
City Hall Parking Lot
CIVIL DETAILS




PROJ:	135-13914-16001
DESN:	GJM
DRWN:	LEM
CHKD:	GJM


C06

Copyright: Tetra Tech

8/5/2019 1:21:38 PM - P:\13914\135-13914-16001\CAD\13513914-16001\EROSION CONTROL PLAN.DWG - MARTINEZ, LESLEY



- NOTES:**
- ① PROTECT EXISTING CB (FIELD VERIFY); SEE 
 - ② PROTECT EXISTING CB PRIOR TO REMOVAL; SEE 
 - ③ INSTALL TEMPORARY SILT FENCE; SEE 

 **EROSION CONTROL PLAN**
SCALE: 1" = 20'

TETRA TECH
www.tetra-tech.com
15350 SW Sequoia Pkwy, Ste 220
Portland, OR 97224
Tel 503.684.9087

MARK	DATE	DESCRIPTION	BY

CITY OF CARLTON
City Hall Parking Lot

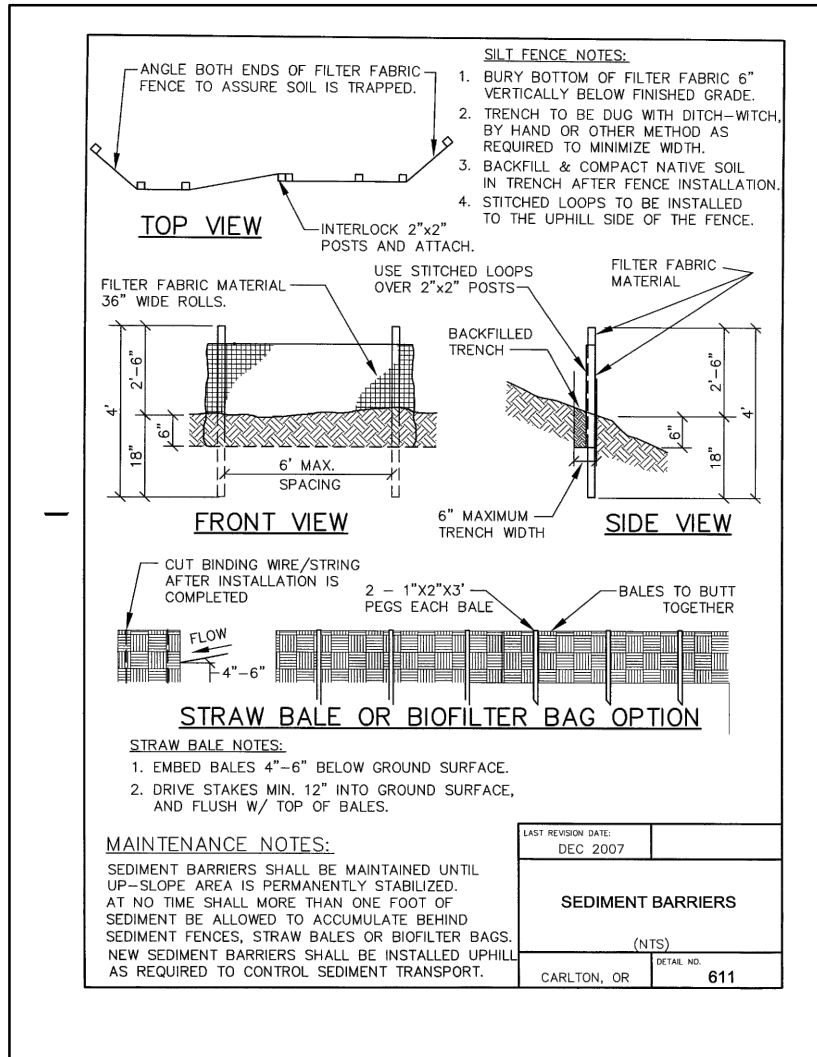
EROSION CONTROL PLAN

PROJ: 135-13914-16001
DESIGN: GJM
DRAWN: LEM
CHKD: GJM

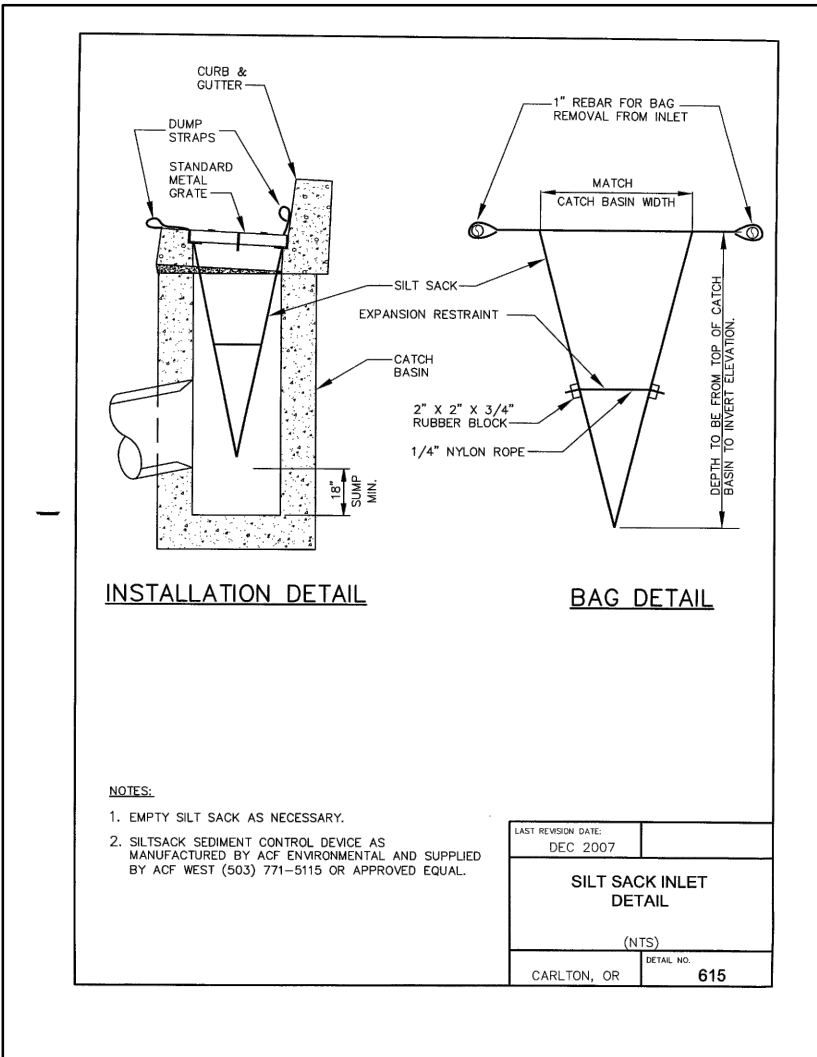
C07

Copyright: Tetra Tech

8/6/2019 1:23:20 PM - P:\13914\135-139-14-16001\CAD\SHETS\CITY HALL PARKING LOT\C08 EROSION CONTROL DETAILS.DWG - MARTINEZ, LESLEY



1 SEDIMENT BARRIERS
SCALE: NONE



2 SILT SACK INLET
SCALE: NONE

MARK	DATE	DESCRIPTION	BY

CITY OF CARLTON
City Hall Parking Lot

EROSION CONTROL DETAILS

PROJ: 135-13914-16001
DESN: GJM
DRWN: LEM
CHKD: GJM

C08



ABBREVIATION DEFINITION

A, AMP	AMPERE
AFG	ABOVE FINISHED GRADE
AHJ	AUTHORITY HAVING JURISDICTION
AIC	AMPERES INTERRUPTING CURRENT
AIL	AMBER INDICATING LIGHT
AUTO	AUTOMATIC
AUX	AUXILIARY
AWG	AMERICAN WIRE GAUGE
BLDG	BUILDING
BM	BEAM MIRROR
BP	BY-PASS CONTACTOR
BT	BEAM TRANSMITTER
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CNTL	CONTROL
CO	CONDUIT ONLY
CONTD	CONTINUED
CR	CONTROL RELAY
CT	CURRENT TRANSFORMER
CU	COPPER
DB	DUCT BANK
DET	DETAIL
DIAG	DIAGRAM
DISC	DISCONNECT
DWG(S)	DRAWING(S)
EL, ELEV	ELEVATION
EMER	EMERGENCY
EQUIP	EQUIPMENT
ETM	ELAPSED TIME METER
FCV	FLOW CONTROL VALVE
FDR	FEEDER
GEN	GENERATOR
GFCI	GROUND-FAULT CIRCUIT INTERRUPTING
GFI	GROUND-FAULT INTERRUPTION
GFP	GROUND-FAULT PROTECTION
GIL	GREEN INDICATING LIGHT
GND	GROUND
GRS	GALVANIZED RIGID STEEL
HH	HAND HOLE
HOA	HAND-OFF-AUTO
HTR	HEATER
I/O	INPUT/OUTPUT
IC	ISOLATION CONTACTOR
IG	ISOLATED GROUND
INST	INSTANTANEOUS
INTLK	INTERLOCK
J	JUNCTION BOX
KCMIL	THOUSAND CIRCULAR MILLS
KV	KILOVOLTS
KVA	KILOVOLT AMPERES (APPARENT POWER)
KVAR	KILOVAR (REACTIVE POWER)
KW	KILOWATTS (REAL POWER)
KWH	KILOWATT HOUR
LS	LEVEL SWITCH
LSH	LEVEL SWITCH HIGH
LT	LEVEL TRANSMITTER
LTG	LIGHTING
mA	MILLIAMPERES
MAN	MANUAL
MCC	MOTOR CONTROL CENTER
MH	MAN HOLE
MTR,M	MOTOR
MTS	MANUAL TRANSFER SWITCH
mV	MILLIVOLTS
N.C.	NORMALLY CLOSED
NF	NON-FUSED
N.O.	NORMALLY OPEN
NO.	NUMBER
NP	NAMEPLATE
NTS	NOT TO SCALE
OL	OVERLOAD
P	PHOTOELECTRIC
PC	PHOTOELECTRIC CONTROL UNIT
PH, Ø	PHASE
PNL	PANEL
PRBP	PRESSURE REDUCING BACKFLOW PREVENTER
PSI	POUNDS PER SQUARE INCH
PVC	POLYVINYL CHLORIDE
REF	REFERENCE
RECP	RECEPTACLE
R/F	RATE OF RISE/FIXED TEMPERATURE
RIL	RED INDICATING LIGHT
RTD	RESISTANCE TEMPERATURE DETECTOR
S, SEC	SECOND
SEL	SELECTOR
SPEC	SPECIFICATION
SPI	SINGLE POINT INSERTION
S.S.	STAINLESS STEEL
SW	SWITCH
SWGR	SWITCH GEAR
TB	TERMINAL BOX, TERMINAL BLOCK
TSP	TWISTED SHIELDED PAIR
TST	TWISTED SHIELDED TRIAD
TYP	TYPICAL
UPS	UNINTERRUPTIBLE POWER SUPPLY
V	VOLTS
Vac or VAC	VOLTS ALTERNATING CURRENT
Vdc or VDC	VOLTS DIRECT CURRENT
VFD	VARIABLE FREQUENCY DRIVE
W	WIRE
W/	WITH
W/O	WITHOUT
WIL	WHITE INDICATING LIGHT
WP	WEATHER PROOF (NEMA 4)
XFMR	TRANSFORMER
XP	EXPLOSION PROOF
ZS	POSITION SWITCH

GENERAL ANNOTATIONS

- KEY NOTES
- CALLOUT - EQUIPMENT
- CALLOUT - CONDUIT

GENERAL WORK DEFINITIONS

- LIGHT LINE INDICATED EXISTING ELECTRICAL OR EXISTING EQUIPMENT. LIGHT LINE MAY ALSO BE USED FOR DETAIL DRAWING CLARITY.
- HEAVY LINE INDICATES NEW WORK

CONDUIT AND WIRING

- CONDUIT IN SLAB OR BELOW GRADE. FIELD WIRING ON CONTROL SCHEMATICS.
- CONDUIT EXPOSED
- CONDUIT BENDS TOWARD OBSERVER
- CONDUIT BENDS AWAY FROM OBSERVER
- CONDUIT STUB
- LIQUID-TITE FLEXIBLE METAL CONDUIT
- PHASE / SWITCHLEG CONDUCTOR
- HOMERUN / CONDUIT
- GROUND CONDUCTOR
- NEUTRAL CONDUCTOR
- PANEL AND CIRCUIT (EXAMPLE: PANEL 2L1, CIRCUITS 1 AND 3)

POWER

- HAND HOLE
- MOTOR CONNECTION
- EQUIPMENT CONNECTION
- DEVICE CONNECTION
- RECEPTACLE - GFCI TYPE
- RECEPTACLE - GFCI TYPE WITH WEATHER-PROOF IN-USE COVER
- SPECIALTY RECEPTACLE
- THERMOSTAT
- DISCONNECT SWITCH
- BRANCH PANELBOARD 120/240V
- UTILITY WATT-HOUR REVENUE METER
- CIRCUIT BREAKER

LIGHTING

- POLE MOUNTED AREA LIGHT
- WALL PACK LIGHT
- LIGHTING FIXTURE IDENTIFIER. *A*-LIGHTING FIXTURE TYPE. SEE LIGHTING FIXTURE SCHEDULE. SURFACE MOUNTED IF NOT SPECIFIED.

CITY HALL PANEL		PANEL SCHEDULE				PROJECT:		Tl Project #135-68830-15001				
120/240V, 1Ph, 3W.		225A BUS		200A M.C.B.		FLUSH MOUNTED		7-Aug-19				
CKT NO	DESCRIPTION/ LOCATION	LOAD (VA)	LOAD TYPE	CB AMP	CB POLE	PHASE	CB AMP	CB POLE	LOAD (VA)	LOAD TYPE	DESCRIPTION/ LOCATION	CKT NO
1	LIGHTS			1	a		1				LIGHTS	2
3	RECEPTICAL KITCHEN			1	b		1				PD LIGHTS	4
5	MANICIPAL COURT			1	a		1				LIGHT MANGER BOTH ROOM LIGHTS	6
7	HVAC ROOF			125	2	b	1				FRONT COUNTER	8
9						a	1				PD PLUG	10
11	ROOF HEAT STRIPS			45	2	b	2				WATER HEATER	12
13						a						14
15	CAR CHARGER			40	2	b	1				SPARE	16
17						a					SPARE	18
19	CAR CHARGER			40	2	b	1				IRRIGATION CONTROLLER	20
21						a					FLOOD LIGHTS, PARKING LIGHTING	22
23	SERVER					b	20	1			COPY, FAX	24
25	ROOF RECEPTACLE					a	2-20	1			POLICE 110	TANDEM CB
27	NEPTUNE TOWER					b	2-20	1			POLICE 220, UPS, SPARE	TANDEM CB
TOT CONN LOAD: Ph A		0 VA		0 A								
TOT CONN LOAD: Ph B		0 VA		0 A								
TOT CONN LOAD:		0 VA		0 A								
MAX PHASE CONN LOAD: Ph A		0 VA										
TOTAL CONNECTED LOAD (3 X MAX):		0.0 KVA		0.0 AMPS					TOTAL DEMAND LOAD:	0.0 KVA	0.0 AMPS	
		CONNECTED LOADS		SUBFED LOADS		TOTAL LOADS	DEMAND FACTOR		DEMAND LOAD			
G	GENERAL (NON-CONT)	0		0		0	100%		0			
L	LIGHTING	0		0		0	125%		0			
R	RECEPTACLES - TO 10 KVA	0		0		0	100%		0		ACCESSORIES	
	OVER 10 KVA	0		0		0	50%		0		<User Defined Additional Panel Requirement>	
K	KITCHEN	0		0		0	100%		0			
H	HEATING	0		0		0	100%		0			
M	MOTORS	0		0		0	100%		0			
LM	LARGEST MOTOR	0		0		0	125%		0			
WH	WATER HEATER	0		0		0	100%		0			
C	CONTINUOUS GENERAL LOAD	0		0		0	125%		0			
N	NON-COINCIDENT	0		0		0	0%		0			
		0		0		0	0%		0			
		0		0		0	0%		0			
		0		0		0	0%		0			
TOTALS		0 VA		0 VA		0 VA			0 VA			

NOTES:
1. PANEL SCHEDULE BASED OFF OF PHOTO TAKEN JULY 2019. UPDATE SCHEDULE WITH BREAKER AMP RATINGS.

1 PANEL SCHEDULE

LUMINAIRE SCHEDULE									
ID	DESCRIPTION	MOUNTING	LAMPS	LUMENS	CCT	CRI ≥	INPUT WATTS	VOLTAGE	FIXTURE TYPE
A	AREA LIGHTING, TYPE "IV" ASYMMETRIC FORWARD, RIGHT SIDE SHIELD CUTOFF DISTANCE 1, GRAY POWDER COATED, 10 INCH ARM, LISTED FOR "WET" LOCATION ROUND STEEL POLE AREA LIGHT ARM MOUNTED AT 30' ABOVE GRADE	POLE	LED	14700	3000	70	116	120	GE LIGHTING - EALS-03 SERIES - OR EQUAL

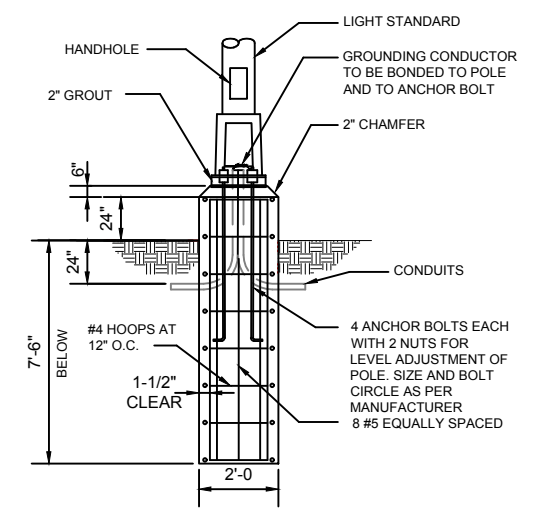
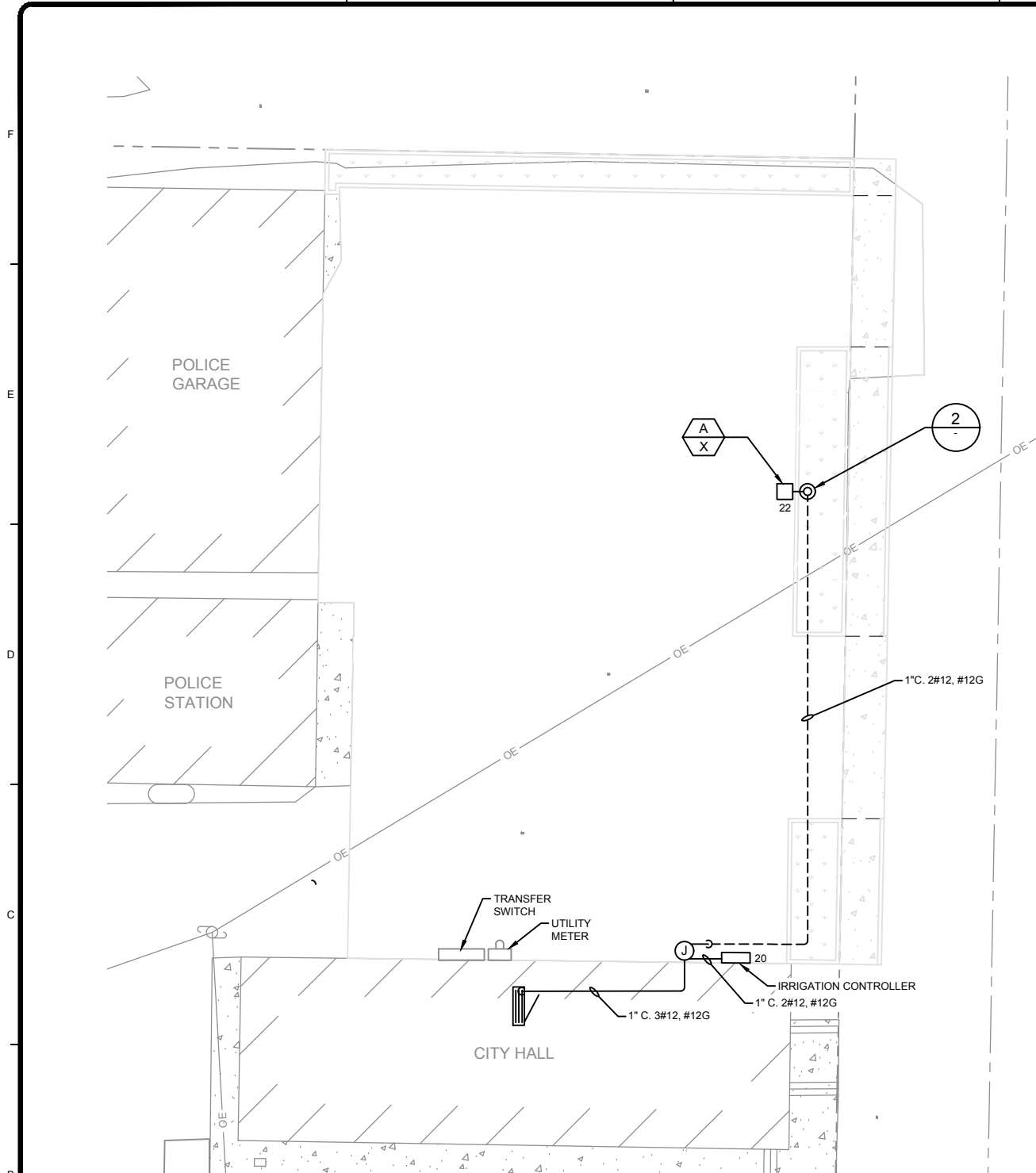
2 LIGHT FIXTURE SCHEDULE

BY									
DATE									
DESCRIPTION									
MARK									
CITY OF CARLTON	City Hall Parking Lot								
ELECTRICAL LEGEND									
PROJ:	135-13914-16001								
DESN:	RBF								
DRWN:	RBF								
CHKD:	GJM								
E01									

NOTES:

1. ON THE ELECTRICAL SERVICE PANELBOARD CONDUCT 30 DAY DEMAND LOAD CALCULATION BASED ON DEMAND ON A 15 MINUTE TIME BASES, PER NEC 220.87, EXCEPTION.
2. CALCULATE AND VERIFY THE ELECTRICAL SERVICE PANELBOARD HAS THE CAPACITY TO ADD THE ADDITIONAL LOAD OF THIS DESIGN. IF THERE ISN'T ADEQUATE CAPACITY LET THE CITY PROJECT MANAGER KNOW AS SOON AS POSSIBLE TO TAKE CORRECTIVE ACTION.
3. PROVIDE RED LINE DRAWING MARKUPS AT COMPLETION OF PROJECT. SHOW ALL CALCULATIONS ON THE RED LINES FOR ITEMS #1 AND #2.
4. IN SERVICE PANELBOARD LOCATE CIRCUIT #22 WHICH FEEDS "FLOOD LIGHTS" AND CALCULATE THE LOAD ON THIS CIRCUIT. IF THERE IS CAPACITY TO ADD THE ADDITIONAL LIGHT OF THIS DESIGN ADD THIS LIGHT TO THE CIRCUIT. IF NOT USE CIRCUIT 28B OF THE TANDEM CIRCUIT BREAKER.
5. UPDATE SERVICE PANELBOARD CIRCUIT DIRECTORY.
6. PROVIDE THE FOLLOWING ITEMS:
 - A. A NEW LIGHTING BRANCH CIRCUIT TO POLE MOUNTED LUMINAIRE.
 - B. LUMINAIRE SHALL BE POLE MOUNTED WITH PHOTOCELL.
 - C. PROVIDE EMT CONDUIT WITH COMPRESSION FITTINGS WITHIN THE CITY HALL BUILDING. RUN THE CONDUIT FROM SERVICE PANELBOARD TO THE EXTERIOR WALL OF THE BUILDING.
 - D. PROVIDE A JUNCTION BOX AND TRANSITION FROM EMT TO PVC COATED RMC CONDUIT.
 - E. MINIMUM CONDUIT SIZE BELOW GRADE 1".
 - F. ALL ELBOWS AND RISERS BELOW GRADE SHALL BE PVC COATED RMC CONDUIT.
 - G. PROVIDE SCHEDULE 80 PVC CONDUIT FOR BELOW GRADE INSTALLATION.
 - H. MINIMUM COVER OF CONDUIT 24" PER NEC TABLE 300.5.
 - I. FINE RAKE BOTTOM OF TRENCH AND REMOVE ALL ROCKS/STONES GREATER THAN 1" FOR A DEPTH OF 2".
 - J. BACKFILL TRENCH WITH NATIVE SOIL WITH ALL ORGANIC AND SHARP MATERIALS REMOVED AND THAT HAS ALL ROCKS/STONES GREATER THAN 1" REMOVE FOR A COVER OF 3" ABOVE THE TOP OF THE CONDUIT.
 - K. PROVIDE COPPER WIRE WITH THWN-2 INSULATION THROUGHOUT.
 - L. PROVIDE NEW BRANCH CIRCUIT TO THE IRRIGATION CONTROLLER.
 - M. PROVIDE RMC CONDUIT BETWEEN THE IRRIGATION CONTROLLER TO THE JUNCTION BOX.
7. AREA LIGHT TO BE MOUNTED 30' AFG. SEE LIGHT FIXTURE SCHEDULE FOR POLE INFORMATION.
7. COORDINATE WITH SITE WORK CONTRACTORS.

8/7/2019 11:21:36 AM - P:\139141\35-13914-1600\1\CAD\SHSHEETFILES\CITY HALL PARKING LOT\E02 POWER PLAN DWG - FORTYGIN, KIRILL



- NOTES:**
1. FOUNDATION TO BEAR AGAINST ORIGINAL SOIL OR 90% COMPACTED BACKFILL OF APPROVED TYPE.

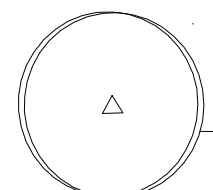
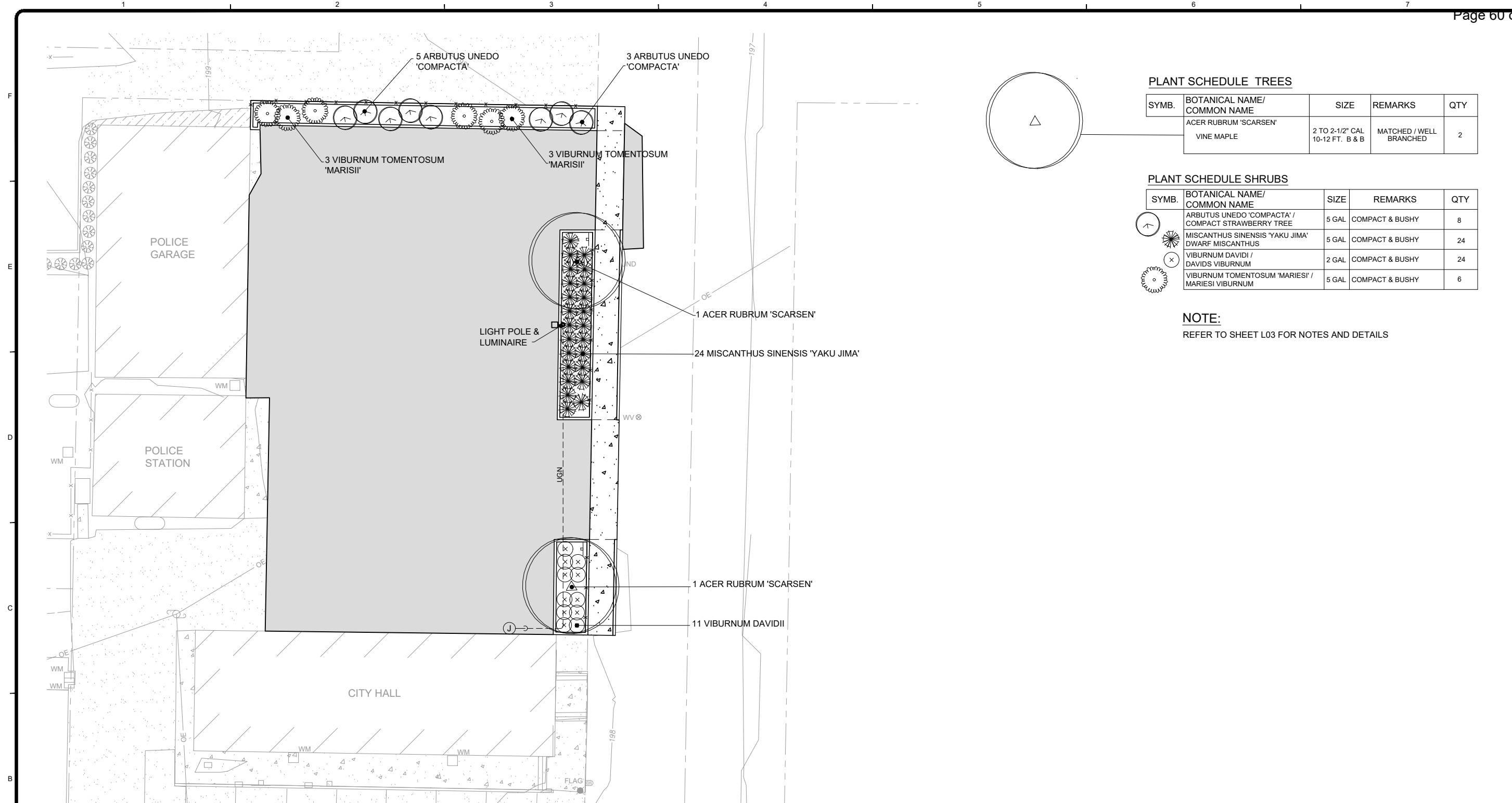
MARK	DATE	DESCRIPTION	BY

CITY OF CARLTON
City Hall Parking Lot
POWER PLAN

PROJ:	135-13914-16001
DESN:	RBF
DRWN:	RBF
CHKD:	GJM

E02

8/5/2019 1:35:08 PM - P:\13914\135-13914-16001\CADD\SHETS\CITY HALL PARKING LOT\01 LANDPLAN.DWG - MARTINEZ, LESLEY



PLANT SCHEDULE TREES

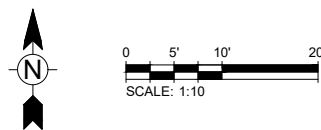
SYMB.	BOTANICAL NAME/ COMMON NAME	SIZE	REMARKS	QTY
△	ACER RUBRUM 'SCARSEN' VINE MAPLE	2 TO 2-1/2" CAL 10-12 FT. B & B	MATCHED / WELL BRANCHED	2

PLANT SCHEDULE SHRUBS

SYMB.	BOTANICAL NAME/ COMMON NAME	SIZE	REMARKS	QTY
⊙	ARBUTUS UNEDO 'COMPACTA' / COMPACT STRAWBERRY TREE	5 GAL	COMPACT & BUSHY	8
⊙	MISCANTHUS SINENSIS 'YAKU JIMA' DWARF MISCANTHUS	5 GAL	COMPACT & BUSHY	24
⊙	VIBURNUM DAVIDI / DAVIDS VIBURNUM	2 GAL	COMPACT & BUSHY	24
⊙	VIBURNUM TOMENTOSUM 'MARIESI' / MARIESI VIBURNUM	5 GAL	COMPACT & BUSHY	6

NOTE:

REFER TO SHEET L03 FOR NOTES AND DETAILS



1 PLANTING PLAN
SCALE: 1" = 10'

TETRA TECH
www.tetratech.com
15350 SW Sequoia Pkwy, Ste 220
Portland, OR 97224
Tel: 503.684.9087

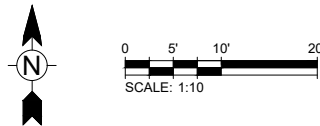
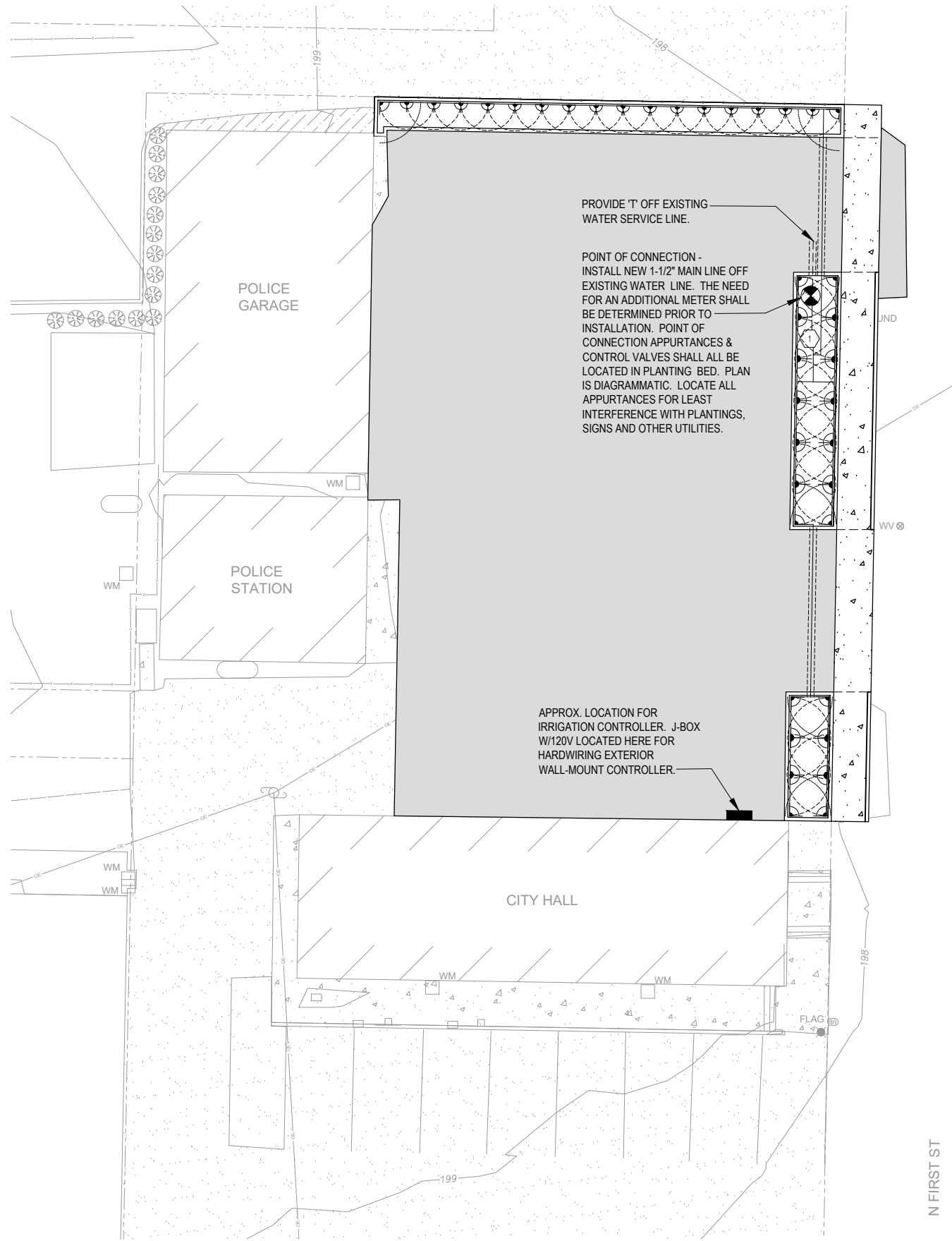
BY	DATE	DESCRIPTION

CITY OF CARLTON
City Hall Parking Lot
PLANTING PLAN

PROJ:	135-13914-16001
DESN:	BAJ
DRWN:	BAJ
CHKD:	GJM

L01

8/9/2019 1:35:14 PM - P:\13914\135-13914-16001\CAD\135-13914-16001\IRRIPLAN.DWG - MARTINEZ, LESLEY



1 IRRIGATION PLAN
SCALE: 1" = 10'

LEGEND

SYMBOL	DESCRIPTION
	1-1/2" MAINLINE (SCHED. 40 PVC)
	POINT OF CONNECTION
	LATERAL LINE (SCHED. 40 PVC, MIN. 1")
	SLEEVE (SCHED. 40 PVC, MIN. 6")
	VALVE
	EXTERIOR WALL MOUNT CONTROLLER

IRRIGATION HEAD SCHEDULE

SYMBOL	NOZZLE & TYPE	GPM	RADIUS
	SPRAY HEAD, ADJUSTABLE ARC 90°	.37	6'
	SPRAY HEAD, ADJUSTABLE ARC 180°	.6	6'

VALVE SCHEDULE

ZONE	SIZE	GPM	TYPE
1	1-1/2"	21.70	SHRUB / SPRAY

PIPE SIZING CHART

GPM	PIPE SIZE
0 - 12 GPM	1 INCH
12-30 GPM	1 1/2 INCH

NOTE:
REFER TO SHEET L03 FOR NOTES AND DETAILS



BY	DATE	DESCRIPTION

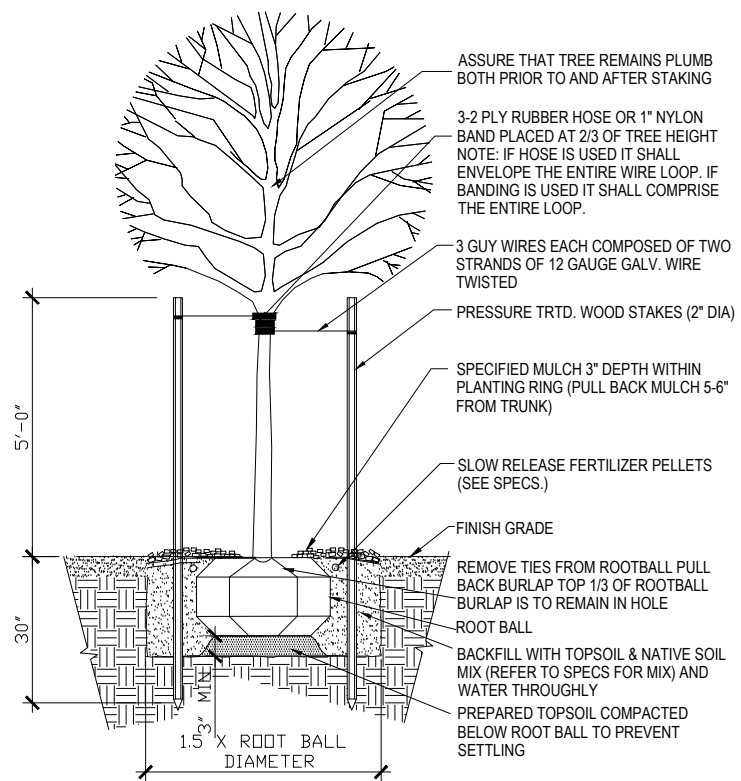
CITY OF CARLTON
City Hall Parking Lot
IRRIGATION PLAN

PROJ:	135-13914-16001
DESIN:	BAJ
DRWN:	BAJ
CHKD:	GJM

L02

TREE PLANTING NOTES:

1. ALL BALLED AND BURLAPPED OR CONTAINER-PACKAGED TREES SHALL BE CAREFULLY HANDLED TO PREVENT DAMAGING AND LOOSENING THE ROOT BALLS. TREES SHALL BE HANDLED AND PLANTED WITHOUT USING THE BRANCHES FOR LIFTING, CARRYING OR SETTING.
2. IF BURLAP IS MADE OF SYNTHETIC FABRIC IT SHOULD BE CAREFULLY REMOVED PRIOR TO BACKFILLING.
3. ALL STAKING SHALL BE REMOVED AT THE END OF PLANT WARRANTY PERIOD (REFER TO SPECIFICATIONS).
4. SEE SPECIFICATIONS FOR ADDITIONAL PLANTING AND SOIL PREPARATION INFORMATION.
5. OVERALL DEPTH VARIES IN PROPORTION WITH SIZE OF ROOT BALL / 24" MINIMUM DEPTH.
6. ASSURE THAT TREE REMAINS PLUMB BOTH PRIOR TO AND AFTER GUYING

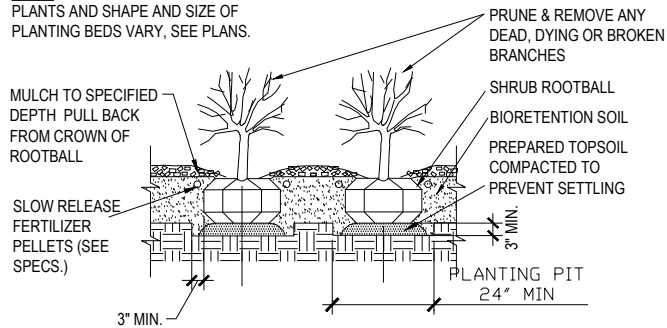


SECTION

1 DECIDUOUS TREE PLANTING DETAILS

L-01 NOT TO SCALE

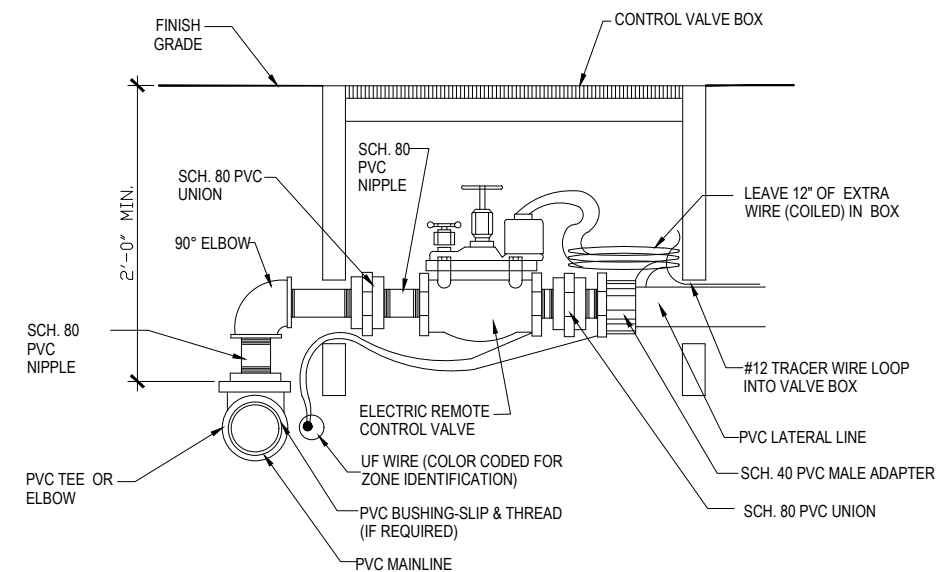
NOTE: ARRANGEMENT AND SPACING OF PLANTS AND SHAPE AND SIZE OF PLANTING BEDS VARY, SEE PLANS.



SECTION

2 SHRUB PLANTING DETAIL

L-01 NOT TO SCALE



NOTES:

1. ALL MAINLINE AND LATERAL PVC PIPE TO BE SCHED. 40
2. CONTROL VALVE TO BE "HUNTER" ICV SERIES (OR APPROVED EQUIVALENT)

3 CONTROL VALVE

L-02 NOT TO SCALE

GENERAL IRRIGATION REQUIREMENTS

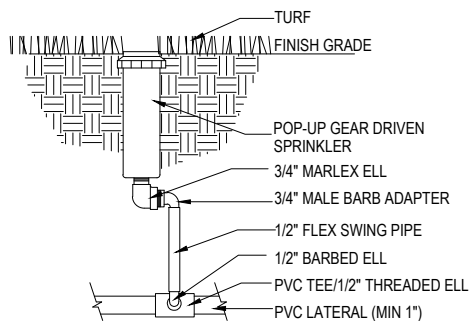
1. THE PLAN IS DIAGRAMMATIC. ALL VALVES & PIPING ARE TO BE INSTALLED IN PLANTING AREAS. PLACE VALVE BOXES ADJACENT TO SIDEWALK WHERE POSSIBLE.
2. PLACE ALL POP-UP SPRINKLERS 2" FROM ALL CURBS AND WALKS AND FLUSH WITH FINISH GRADE.
3. PROTECT ALL UTILITIES ENCOUNTERED THROUGH THE SCOPE OF WORK.
4. ADJUST NOZZLES TO MAXIMIZE COVERAGE AND MINIMIZE OVERSPRAY. ALL HEADS SHOWN IN EACH ZONE TO BE NOZZLED AND ADJUSTED PER MANUFACTURER'S SPECIFICATIONS FOR MATCHED PRECIPITATION RATES.
5. PROVIDE NECESSARY SLEEVING UNDER ALL ROADS, DRIVES AND WALKWAYS WHETHER SPECIFICALLY INDICATED ON PLANS OR NOT. MARK LOCATION OF SLEEVES AND TAPE ENDS. EXTEND SLEEVE MIN. 6" BEYOND EDGE OF PAVING. MIN. COORDINATE SLEEVING WITH ALL UTILITIES ENCOUNTERED THROUGH THE SCOPE OF WORK. SLEEVING SHALL BE 4" DIA SCHED 40 UNLESS NOTED OTHERWISE.
6. LOCATE PIPE NEAR PERMANENT STRUCTURES WHERE POSSIBLE, I.E. ALONG EDGES OF SIDEWALKS. LEAVE PIPE SIZING FACE UP TO FACILITATE REPAIR.
7. AN AUTOMATIC CONTROLLER SHALL BE LOCATED IN AREA DIRECTED BY OWNER'S REPRESENTATIVE. CONTROLLER LOCATION SHALL BE COORDINATED BY ELECTRICAL CONTRACTOR. ELECTRICAL SERVICE AND CONDUIT TO CONTROLLER SHALL BE PROVIDED. CONTROLLER INPUT IS 120/240 VAC, 50/60 Hz.
8. POC TO BE INSTALLED AT OFF WATER MAIN IN APPROXIMATE LOCATION SHOWN ON PLAN. PROVIDE SHUT-OFF VALVE, LEGAL BACKFLOW PREVENTER.
9. PIPE SHALL CONFORM TO THE REQUIREMENTS OF ASTM D 1785, PVC 1120 SCHEDULE 40; OR ASTM D 2241, PVC 1120 SDR 21, CLASS 200. PVC PIPE SHALL BE USED FOR ALL MAINLINE AND LATERAL PIPING AND SIZED AS SHOWN ON THE DRAWINGS. SCHEDULE 40 PVC IS TO BE USED FOR ALL MAINLINE PIPING AND SLEEVING. CLASS 200 PVC IS TO BE USED FOR ALL LATERAL PIPING. ALL PVC PIPE SHALL BE CONTINUOUSLY AND PERMANENTLY MARKED WITH MANUFACTURER'S NAME, MATERIAL AND SCHEDULE OR TYPE.
10. PVC FITTINGS: SOLVENT WELDED SOCKET TYPE FITTINGS SHALL CONFORM TO REQUIREMENTS OF ASTM D 2466, SCHEDULE 40. THREADED TYPE FITTINGS SHALL CONFORM TO REQUIREMENTS OF ASTM D 2464, SCHEDULE 80.
11. POP-UP SPRAY HEADS: SPRINKLERS SHALL HAVE BODY MATERIAL AND NOZZLES OF ABS SPRING TO BE STAINLESS STEEL AND ALL INTERNAL PARTS TO BE CORROSION RESISTANT. PATTERN TO BE FIXED WITH FLOW ADJUSTMENT. SPRAY HEADS TO BE HUNTER, RAINBIRD OR APPROVED EQUIVALENT.
12. REMOTE CONTROL VALVES, ELECTRICAL: REMOTE CONTROL VALVES SHALL BE SOLENOID ACTUATED GLOBE VALVES OF 1 INCH SIZE, SUITABLE FOR 24 VOLTS, 60 CYCLE, AND DESIGNED TO PROVIDE FOR SHUT-OFF IN EVENT OF POWER FAILURE. VALVE SHALL BE CAST BRONZE OR BRASS OR PLASTIC HOUSING SUITABLE FOR SERVICE AT 150 PSI OPERATING PRESSURE WITH EXTERNAL FLOW CONTROL ADJUSTMENT FOR SHUT-OFF CAPABILITY. EXTERNAL PLUG AT DIAPHRAGM CHAMBER TO ENABLE MANUAL OPERATION, FILTER IN CONTROL CHAMBER TO PREVENT VALVE BODY CLOGGING WITH DEBRIS, DURABLE DIAPHRAGM, AND ACCESSIBILITY TO INTERNAL PARTS WITHOUT REMOVING VALVE FROM SYSTEM.
13. CONTROLLER SHALL CONFORM TO THE REQUIREMENTS OF NEMA ICS 2 WITH 120-VOLT SINGLE PHASE SERVICE, OPERATING WITH INDICATED STATIONS, AND GROUNDED CHASSIS. ENCLOSURE SHALL CONFORM TO NEMA ICS 6 TYPE 3R, WITH LOCKING STAINLESS STEEL HINGED COVER AND WALL-MOUNTED. CONTROLLER TO BE HUNTER XC HYBRID 6 STATION EXTERIOR OR APPROVED EQUIVALENT.
- 14.

GENERAL PLANTING NOTES

1. QUALITY STANDARDS: AS DESCRIBED IN THE CURRENT EDITION OF "AMERICAN STANDARD FOR NURSERY STOCK" ISSUED BY THE AMERICAN NATIONAL STANDARDS INSTITUTE AND HEREINAFTER CALLED ANSI Z60.1.
2. ALL PLANTING AREAS TO RECEIVE A MINIMUM 6" OF 3 WAY MIX OF EQUAL PARTS COURSE SAND, COMPOST AND LOAM. SUBGRADE OF PLANTING BEDS SHALL BE ROTOTILLED TO A DEPTH OF 6" PRIOR TO APPLICATION OF TOPSOIL. TOPSOIL TO BE APPLIED IN TWO 3" LIFTS. THE INITIAL LIFT TO BE ROTOTILLED INTO THE SUBGRADE.
3. BARK MULCH SHALL BE USED AS TOP DRESSING IN ALL SHRUB PLANTING BEDS TO A DEPTH OF 3".
4. FINISH GRADES IN PLANTING BEDS SHALL BE 1" BELOW TOP OF PAVEMENT OR CURB.
5. PRIOR TO TREE PLANTING ALL UNDERGROUND UTILITIES SHALL BE LOCATED. TREES TO BE PLANTED NO CLOSER (CENTER OF ROOTBALL) THAN 5' FROM ANY NEW OR EXISTING UTILITY LINES OR MANHOLES OR LIGHT POLES.
6. ALL AREAS TO BE CLEAN AND FREE OF DEBRIS UPON COMPLETION OF PROJECT. DURING PLANTING, KEEP ADJACENT PAVING AND CONSTRUCTION CLEAN AND WORK AREA IN AN ORDERLY CONDITION. CLEAN WHEELS OF VEHICLES BEFORE LEAVING SITE TO AVOID TRACKING SOIL ONTO ROADS, WALKS, OR OTHER PAVED AREAS.
7. ALL PLANT MATERIAL AND LAYOUT OF PLANTS TO BE APPROVED BY OWNER'S REPRESENTATIVE PRIOR TO PLANTING. PROVIDE OWNER'S REPRESENTATIVE WITH A MINIMUM THREE (3) WORKING DAYS' NOTICE AS TO WHEN PLANT MATERIALS, OR PLANT LAYOUT, AS APPLICABLE, WILL BE READY FOR REVIEW ON SITE. OWNER'S REPRESENTATIVE RESERVES THE RIGHT TO REJECT PLANT MATERIALS FOR NONCONFORMANCE AT ANY TIME FROM THE DELIVERY OF SUCH PLANT MATERIAL TO THE SITE THROUGH THE END OF THE WARRANTY PERIOD. REMOVE SURPLUS SOIL AND WASTE MATERIAL INCLUDING EXCESS SUBSOIL, UNSUITABLE SOIL, TRASH, AND DEBRIS AND LEGALLY DISPOSE OF THEM OFF OWNER'S PROPERTY.
8. MAINTENANCE SCHEDULE: PLANT AND GROUND COVER BEDS ARE TO BE WEED FREE AT THE END OF EACH FOUR (4) WEEK PERIOD DURING THE MAINTENANCE PERIOD. PROVIDE WRITTEN NOTICE TO OWNER'S REPRESENTATIVE OF THE COMPLETION OF EACH WEEDING OPERATION. OWNER'S REPRESENTATIVE WILL PERIODICALLY REVIEW LANDSCAPE MAINTENANCE PRACTICES UNTIL FINAL ACCEPTANCE. DEFICIENCIES WILL BE NOTED AND REPORTED TO THE CONTRACTOR. CORRECT DEFICIENCIES TO THE SATISFACTION OF THE OWNER'S REPRESENTATIVE WITHIN TWO WEEKS' NOTICE OF DEFICIENCIES. MAINTENANCE PERIOD WILL EXTEND UNTIL FINAL ACCEPTANCE BY THE OWNER'S REPRESENTATIVE.
9. WARRANT THE WORK OF THIS SECTION FOR A PERIOD OF ONE (1) YEAR FROM THE DATE OF FINAL ACCEPTANCE AGAINST DEFECTS OF MATERIALS AND WORKMANSHIP (THE WARRANTY PERIOD). DURING THE WARRANTY PERIOD, MAKE REPLACEMENTS OF PLANT MATERIALS WITHIN THIRTY (30) DAYS OF AWARENESS OF PLANT DEATH OR ABNORMAL GROWING CONDITION. PLANTS DAMAGED BY VANDALISM AFTER FINAL ACCEPTANCE OR RESULTING FROM DAMAGE BY OWNER'S OCCUPANCY OF THE SITE WILL NOT REQUIRE REPLACEMENT, UNLESS IMPROPER INSTALLATION IS A CONTRIBUTING FACTOR IN THE DAMAGE, INCLUDING IMPROPER STAKING, PLANT PIT SIZE OR PROTECTION. PLANT REPLACEMENT DURING THE WARRANTY PERIOD WILL BE LIMITED TO ONE (1) REPLACEMENT PER PLANT UNLESS REPEATED FAILURE IS DUE TO IMPROPER SOIL PREPARATION, PLANT INSTALLATION OR THE FAILURE OF PLANTS TO MEET THE SPECIFICATIONS OF THIS SECTION. REPLACE PLANTS NOT IN NORMAL HEALTHY GROWING CONDITION AT END OF THE WARRANTY PERIOD AS DETERMINED BY OWNER'S REPRESENTATIVE. REPLACE WITH PLANTS OF IDENTICAL SPECIES AND SIZE. ONE-YEAR WARRANTY REVIEW: WARRANTY REVIEW SHALL BE SCHEDULED BY THE CONTRACTOR WITH THE OWNER A MINIMUM OF THIRTY (30) DAYS PRIOR TO THE END OF THE ONE-YEAR WARRANTY PERIOD. THE CONTRACTOR WILL NOTIFY PARTIES OF THE WARRANTY REVIEW A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO THE ANTICIPATED MEETING DATE.

4 POP-UP SPRAY HEAD

L-02 NOT TO SCALE



MARK	DATE	DESCRIPTION	BY

CITY OF CARLTON
City Hall Parking Lot
LANDSCAPE NOTES & DETAILS

PROJ:	135-13914-16001
DESN:	BAJ
DRWN:	BAJ
CHKD:	GJM

L03



This page intentionally left BLANK.

POLE BUILDING PLANS

BUILDING OWNER: CITY OF CARLTON
BUILDING LOCATION: 191 E MAIN ST.
 CARLTON, OR, 97111
COUNTY: YAMHILL
BUILDING DIMENSIONS: 30' X 48' X 14'
ROOF PITCH: 4 IN 12
BUILDING AREA/CONST. TYPE: 1,440 SF / TYPE V-B
ROOF FRAMING SYSTEM: PRE-ENGINEERED TRUSSES BY OTHERS
ROOF ASSEMBLY: METAL OVER WOOD SHEATHING
WALL ASSEMBLY: METAL
BUILDING USE: PRIVATE SHOP

CONTRACTOR/BUILDER: BARKER BUILDINGS INC
ADDRESS: P.O. BOX 194
 LAFAYETTE, OR, 97127

BUILDING DESIGN CRITERIA

GROUND SNOW LOAD = 25 PSF
 ROOF DEAD LOAD = 5 PSF
 WIND SPEED = 100 MPH (3 SEC. GUST)
 WIND EXPOSURE = B
 SEISMIC DESIGN PARAMETERS:
 $S_s = 0.87$; $S_1 = 0.40$
 SEISMIC DESIGN CATEGORY = D
 RISK CATEGORY = II
 SOIL BEARING CAPACITY = 1,500 PSF

BUILDING CODES & REFERENCES

-2019 OSSC
 -ASCE 7-16
 -NDS 2012

SHT	DRAWING LIST
1	C1.0 COVER SHEET
2	P1.0 PLAN VIEW
3	E1.0 ELEVATION VIEWS
4	S1.0 SECTIONS & DETAILS
5	D1.0 POSTHOLE, PURLIN & GIRT DETAILS
6	D2.0 SHEATHING DETAILS


CONSTRUCTION NOTES

- ALL DRAWINGS SHALL BE REVIEWED AND APPROVED BY OWNER AND/OR CONTRACTOR PRIOR TO BEGINNING OF SITE WORK OR BUILDING CONSTRUCTION. ALL DIMENSIONS AND SPECIFICATIONS SHALL BE VERIFIED AND ANY DISCREPANCIES, ERRORS AND/OR OMISSIONS SHALL BE REPORTED TO SOUTH VALLEY ENGINEERING PRIOR TO MATERIAL PURCHASE AND BEGINNING OF CONSTRUCTION. SOUTH VALLEY ENGINEERING SHALL NOT BE HELD LIABLE FOR ANY COSTS OR DAMAGES INCURRED DUE TO DISCREPANCIES, ERRORS AND/OR OMISSIONS DISCOVERED AFTER CONSTRUCTION HAS BEGUN.
- THE BUILDING OWNER AND OR CONTRACTOR ACCEPTS FULL RESPONSIBILITY FOR ANY WORK DONE THAT IS NOT SHOWN ON OR DOES NOT COMPLY WITH THESE PLANS. REQUESTS FOR ANY CHANGES SHALL BE MADE TO THE ENGINEER IN WRITING, AND SHALL BE RECEIVED FROM THE ENGINEER IN WRITING PRIOR TO IMPLEMENTATION.
- STRUCTURAL CONCRETE FOR SLABS AND FOUNDATIONS SHALL BE A MINIMUM OF 2,500 PSI COMPRESSIVE STRENGTH AT 28 DAYS WITH NO SPECIAL INSPECTION REQUIRED. FOOTINGS AND FOUNDATIONS SHALL BE CAST AGAINST UNDISTURBED NATIVE SOIL UNLESS OTHERWISE NOTED ON THE PLANS.
- ALL DIMENSIONAL LUMBER SHALL BE #2 DF OR BETTER UNLESS OTHERWISE NOTED ON THE PLANS. HIGHER GRADE LUMBER OF SAME SPECIES MAY BE SUBSTITUTED FOR SPECIFIED GRADE.
- ALL WOOD IN CONTACT WITH CONCRETE ABOVE GROUND SHALL BE PRESSURE TREATED FOR ABOVE GROUND CONTACT. ALL WOOD EMBEDDED IN GROUND SHALL BE PRESSURE TREATED FOR BURIAL.
- ROOF & WALL WOOD SHEATHING (IF USED) UNLESS OTHERWISE NOTED ON THE PLANS:**
 ALL ROOF AND WALL WOOD SHEATHING SHALL BE AN APPROVED APA RATED SHEATHING, EXPOSURE I, GRADE C-D, EXTERIOR GLUE, 7/16" THICK (MINIMUM), EXTERIOR GRADE SHEATHING SHALL BE RATED FOR EXTERIOR USE. WOOD ROOF SHEATHING SHALL BE NAILED WITH 8d NAILS AT 6" O.C. EDGES AND 12" O.C. FIELD. 2X BLOCKING IS NOT REQUIRED FOR ROOF SHEATHING. WALL SHEATHING SHALL BE NAILED WITH 8d NAILS AT 6" O.C. EDGES AND 12" O.C. FIELD OR AS SPECIFIED ON THE PLANS. 2X BLOCKING SHALL BE INSTALLED AT ALL PANEL EDGES ON ALL WALLS.
- ALL FASTENERS EXPOSED TO THE ELEMENTS SHALL BE GALVANIZED OR CORROSION RESISTANT. ALL FASTENERS IN PRESSURE TREATED WOOD SHALL BE HOT-DIPPED GALVANIZED OR STAINLESS STEEL.
- IF PLANS SPECIFY GRANULAR BACKFILL IN POSTHOLE, BACKFILL WITH 3/4" MINUS CLEAN CRUSHED GRAVEL TO SPECIFIED EMBEDMENT DEPTH, BACKFILL IN 6" LIFTS AND SATURATE AND COMPACT EACH LIFT. NO SPECIAL INSPECTION REQUIRED.
- UNLESS OTHERWISE NOTED, THE 6" THICK PAD AT THE BOTTOM OF THE POST HOLES IS NOT REQUIRED FOR DOOR POSTS NOT SUPPORTING VERTICAL LOADS, AND MAY BE OMITTED FOR STRUCTURAL POSTS BEARING ON SOLID ROCK.
- INSTALL ALL TRUSS BRACING PER TRUSS ENGINEERING. INSTALL TEMPORARY TRUSS BRACING DURING CONSTRUCTION PER BCSEB10 GUIDELINES FOR POST FRAME TRUSS INSTALLATION, RESTRAINT & BRACING DURING CONSTRUCTION.

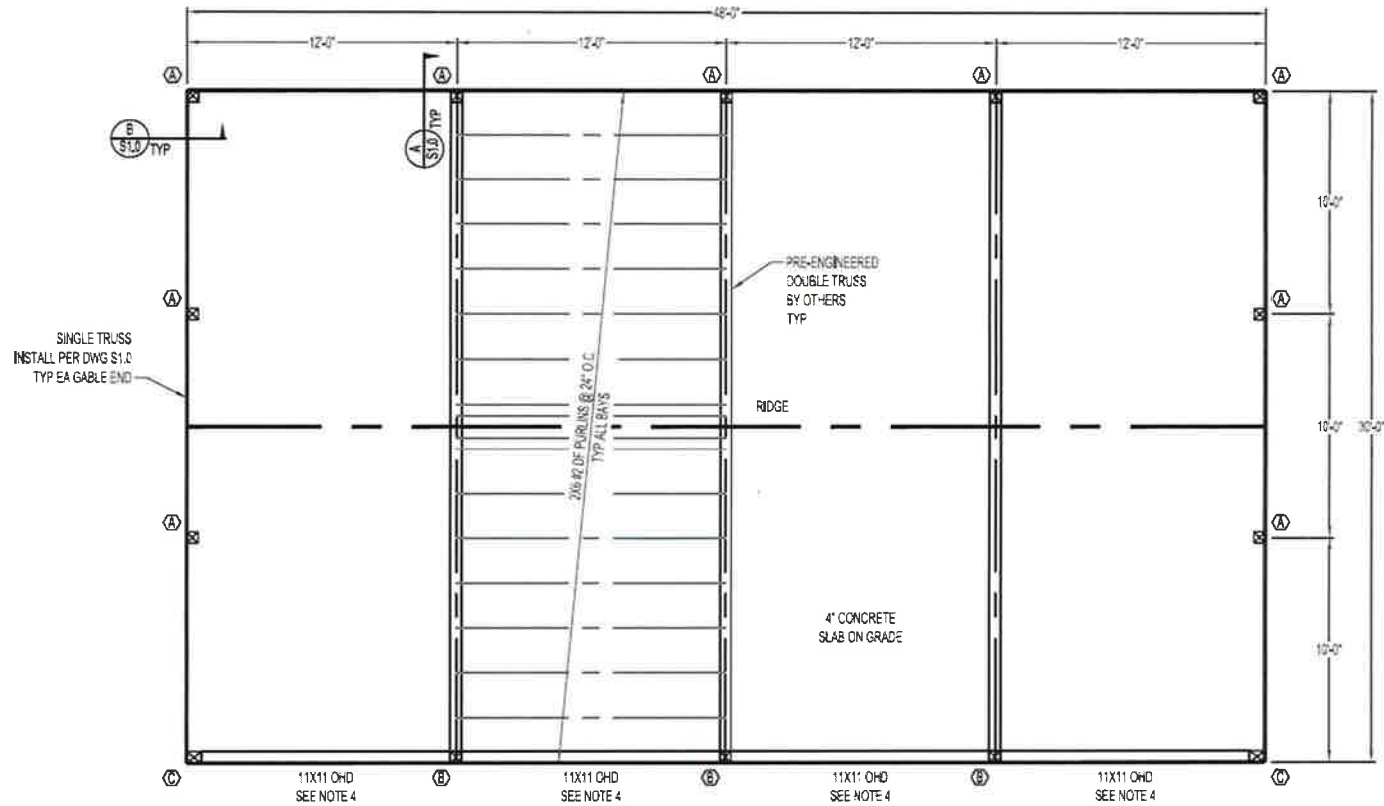
THE PLANS FOR THIS BUILDING ARE THE PROPERTY OF SOUTH VALLEY ENGINEERING, LLC AND SHALL BE SURRENDERED UPON REQUEST. THIS BUILDING HAS BEEN ENGINEERED TO CONFORM TO THE DIMENSIONS AND BUILDING DESIGN CRITERIA SPECIFIED, AND SHALL BE BUILT ONLY AT THE LOCATION DESIGNATED ON THESE PLANS. ANY CHANGES OR MODIFICATIONS TO THESE PLANS, INCLUDING BUT NOT LIMITED TO BUILDING DIMENSIONS, DESIGN CRITERIA, OR BUILDING LOCATION WITHOUT EXPRESS WRITTEN PERMISSION OR DOCUMENTATION BY THE ENGINEER WHOSE SEAL IS AFFIXED TO THESE PLANS SHALL RENDER THESE PLANS NULL AND VOID, AND THE ENGINEER WHOSE SEAL IS AFFIXED TO THESE PLANS SHALL NOT BE HELD LIABLE FOR THE STRUCTURAL INTEGRITY OR CODE COMPLIANCE OF ANY BUILDING CONSTRUCTED REFERENCING THESE PLANS UNDER THESE CONDITIONS.



COVER SHEET			
DRAWING NO.: C1.0	OWNER: CITY OF CARLTON		
REV.: 0	SHEET: 1	LOCATION: 191 E MAIN ST.	
PROJECT NO.: 12101014		CARLTON, OR, 97111	
DATE: 28 JAN 2021	SCALE: N/A	DRAWN BY: SA	



South Valley Engineering
 4742 Liberty Rd. S #151 • Salem, OR, 97302
 Ph. (503) 302-7020 • Fax (888) 535-6341
www.southvalleyengineering.com



GENERAL NOTES

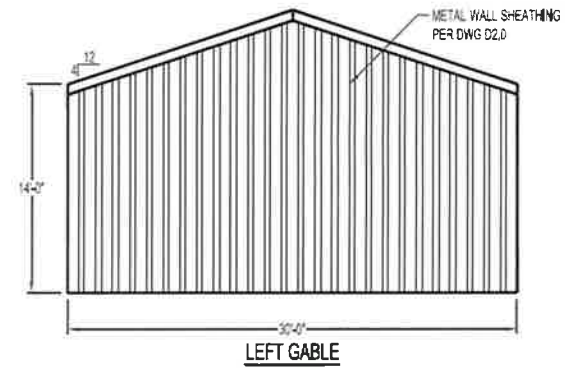
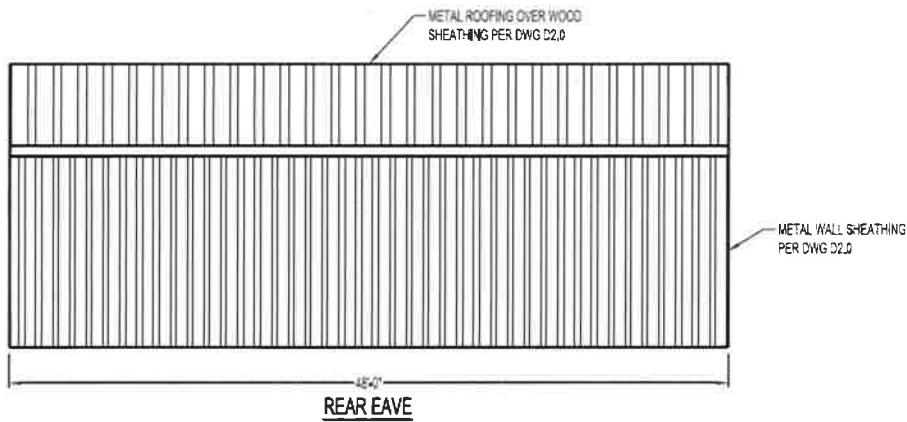
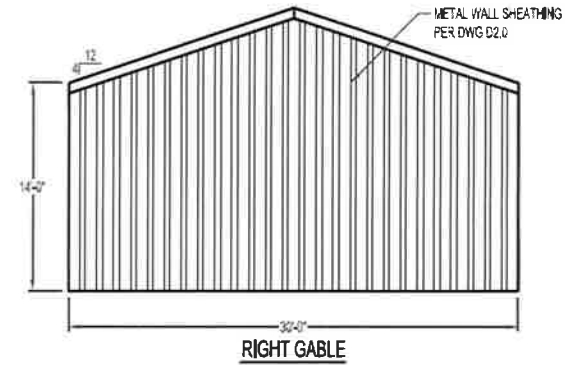
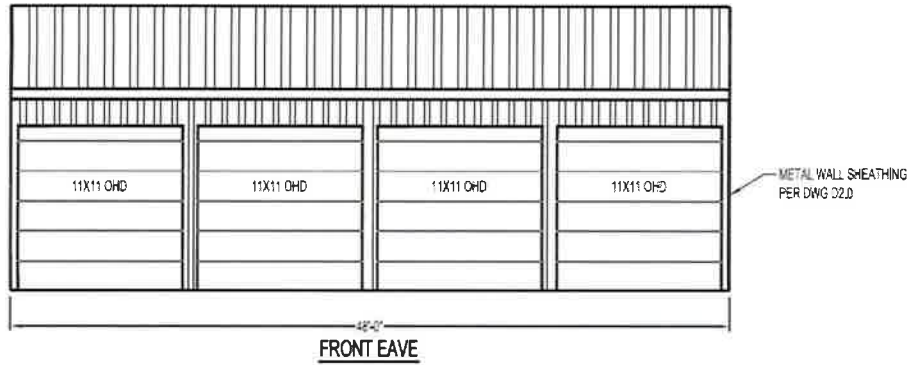
1. ALL POSTS TO BE #2 HF PRESSURE TREATED FOR BURIAL AND ORIENTED AS SHOWN UNLESS OTHERWISE NOTED
2. UNLESS OTHERWISE DIMENSIONED, ALL WINDOWS AND WALK DOORS MAY BE LOCATED BY THE OWNER/CONTRACTOR IN THE WALLS SHOWN
3. EMBED POSTS AND BACKFILL PER CONTRACTOR. CONCRETE BACKFILL MAY BE SUBSTITUTED FOR GRANULAR BACKFILL
4. CONTRACTOR TO VERIFY OVERHEAD DOOR DIMENSIONS AND CLEARANCES PRIOR TO BUILDING CONSTRUCTION AND OVERHEAD DOOR INSTALLATION

TABLE 1

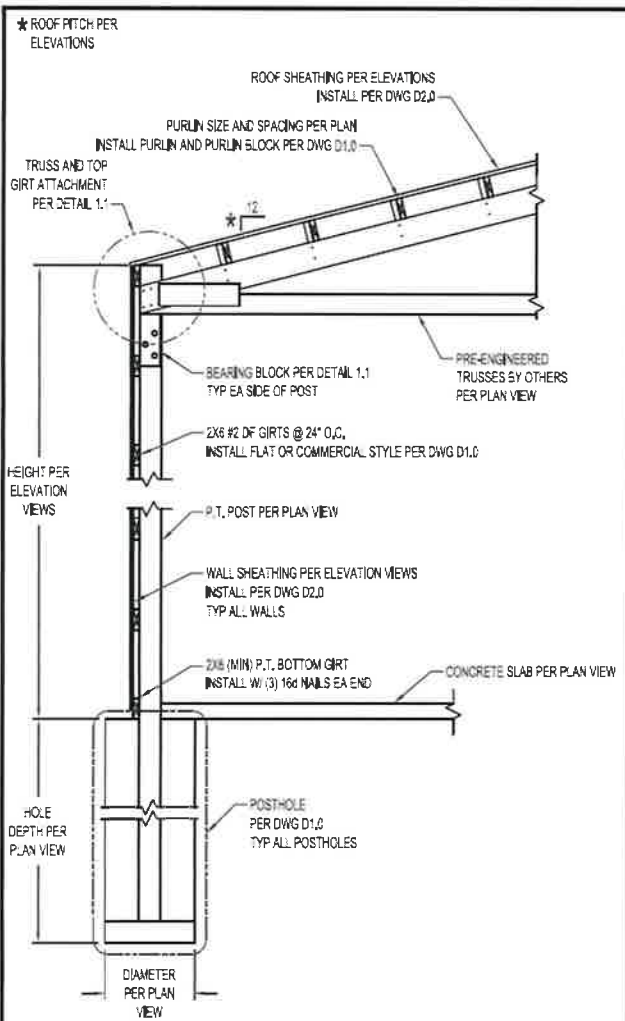
POST	SIZE	HOLE DIMENSIONS		BACKFILL	NOTES
		DIAMETER	DEPTH		
(A)	6X6	24"Ø	4'-6"	GRANULAR	-
(B)	6X6	24"Ø	4'-6"	CONCRETE	-
(C)	6X8	24"Ø	4'-6"	CONCRETE	ORIENT PARALLEL WITH EAVE WALL



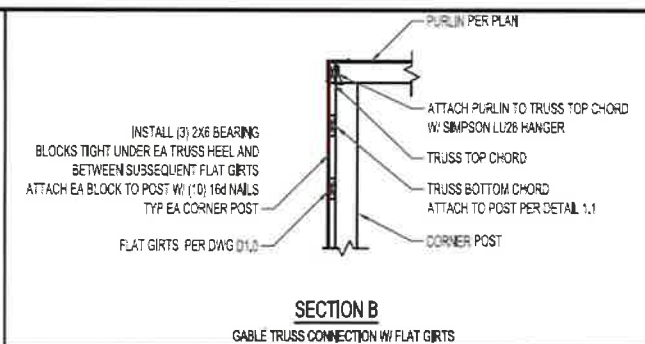
PLAN VIEW			
DRAWING NO.: P1,0	OWNER: CITY OF CARLTON		
REV.: 0	SHEET: 2	LOCATION: 191 E MAIN ST.	
PROJECT NO.: 12101014		CARLTON, OR. 97111	
DATE: 28 JAN 2021	SCALE: 3/16" = 1'-0"	DRAWN BY: SA	
		South Valley Engineering 4742 Liberty Rd. S #151 • Salem, OR. 97302 Ph. (503) 302-7020 • Fax (888) 535-6341 www.southvalleyengineering.com	



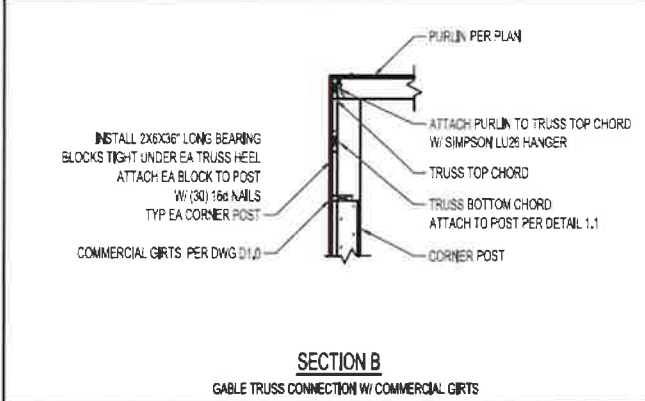
<p>REGISTERED PROFESSIONAL ENGINEER OREGON JULY 9, 2021 STEPHEN R. HERYFORD REVIEWS: 5/30/21</p>	ELEVATION VIEWS		
	DRAWING NO.: E1.0	OWNER: CITY OF CARLTON	
	REV.: 0	SHEET: 3	LOCATION: 191 E MAIN ST.
	PROJECT NO.: 12101014	CARLTON, OR. 97111	
DATE: 28 JAN 2021	SCALE: 1/8" = 1'-0"	DRAWN BY: SA	
<p>South Valley Engineering 4742 Liberty Rd. S #151 • Salem, OR. 97302 Ph. (503) 302-7020 • Fax (888) 535-6341 www.southvalleyengineering.com</p>			



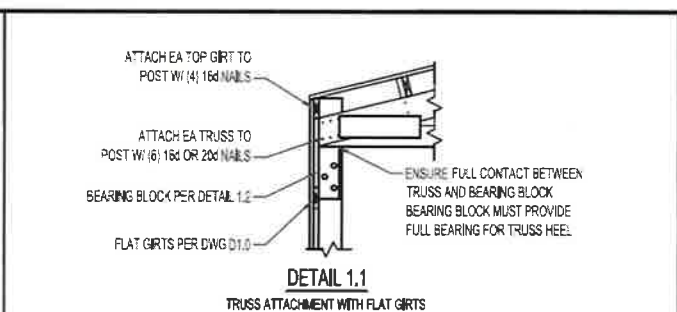
SECTION A



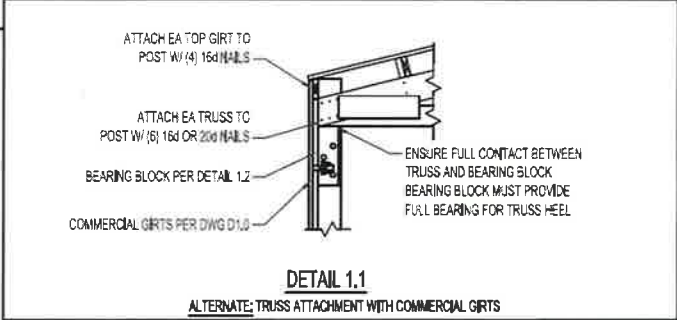
SECTION B
GABLE TRUSS CONNECTION W/ FLAT GIRTS



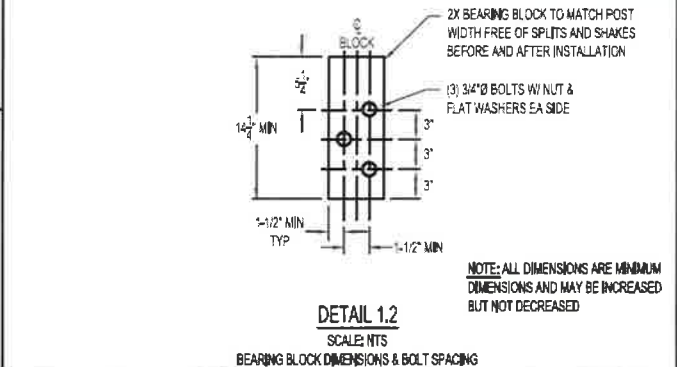
SECTION B
GABLE TRUSS CONNECTION W/ COMMERCIAL GIRTS



DETAIL 1.1
TRUSS ATTACHMENT WITH FLAT GIRTS



DETAIL 1.1
ALTERNATE: TRUSS ATTACHMENT WITH COMMERCIAL GIRTS



DETAIL 1.2
SCALE: NTS

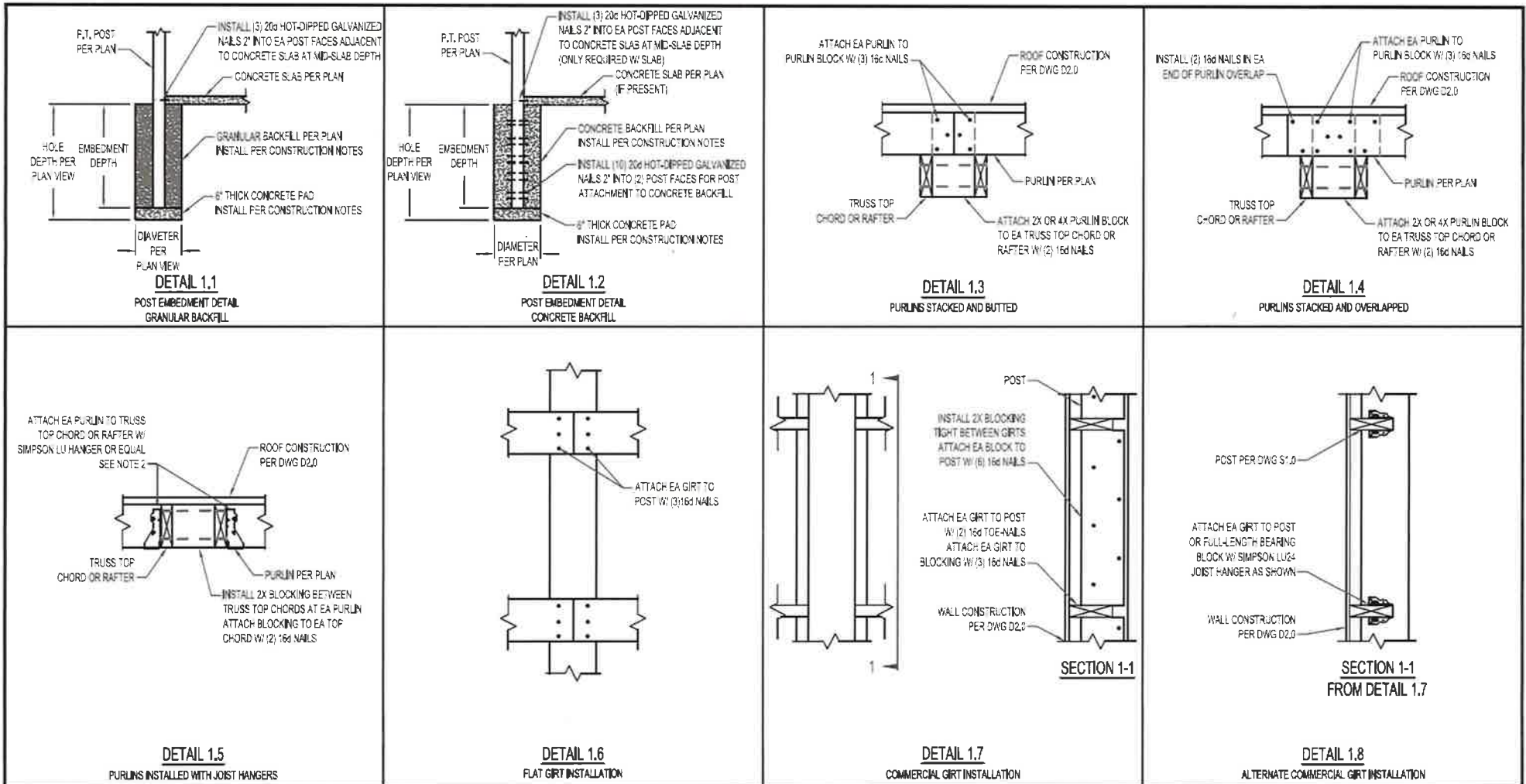
BEARING BLOCK DIMENSIONS & BOLT SPACING

NOTE: ALL DIMENSIONS ARE MINIMUM DIMENSIONS AND MAY BE INCREASED BUT NOT DECREASED

SECTIONS & DETAILS			
DRAWING NO.: S1.0	OWNER: CITY OF CARLTON		
REV.: 0	SHEET: 4	LOCATION: 191 E MAIN ST.	
PROJECT NO.: 12101014	CARLTON, OR. 97111		
DATE: 28 JAN 2021	SCALE: 3/8"=1'-0"	DRAWN BY: SA	

RENEWS: 6/30/21

South Valley Engineering
4742 Liberty Rd. S #151 • Salem, OR. 97302
Ph. (503) 302-7020 • Fax (503) 535-6341
www.southvalleyengineering.com



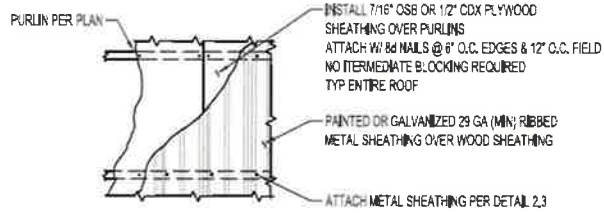
- GENERAL NOTES**
1. ALL NAILS INTO PRESSURE TREATED LUMBER SHALL BE HOT-DIPPED GALVANIZED
 2. IF PURLIN DEPTH EXCEEDS TRUSS TOP CHORD DEPTH, USE SIMPSON JB TOP FLANGE HANGER OR EQUAL



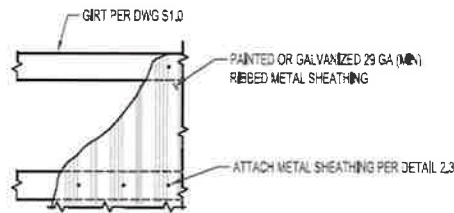
POSTHOLE, PURLIN & GIRTS DETAILS

DRAWING NO.: D1.0	OWNER: CITY OF CARLTON
REV.: 0	SHEET: 5
PROJECT NO.: 12101014	LOCATION: 191 E MAIN ST. CARLTON, OR, 97111
DATE: 28 JAN 2021	SCALE: NTS
	DRAWN BY: SA

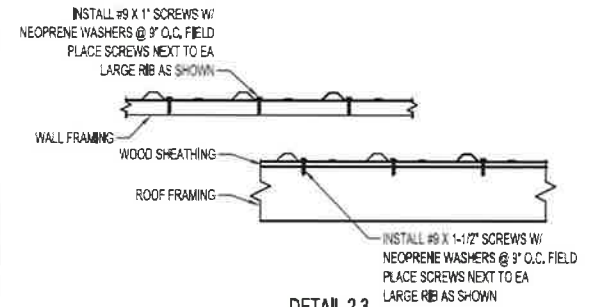
South Valley Engineering
4742 Liberty Rd. S #151 • Salem, OR, 97302
Ph. (503) 302-7020 • Fax (888) 535-6341
www.southvalleyengineering.com



DETAIL 2.1
ROOF SHEATHING



DETAIL 2.2
WALL SHEATHING



DETAIL 2.3
SCALE X2
METAL SHEATHING ATTACHMENT
STANDARD SCREW SCHEDULE

GENERAL NOTES

1. FLAT GIRTS ARE SHOWN FOR WALL CONSTRUCTION. WALL SHEATHING INSTALLATION IS SIMILAR FOR COMMERCIAL GIRTS.

SHEATHING DETAILS			
DRAWING NO.: D2.0	OWNER: CITY OF CARLTON		
REV.: 0	SHEET: 6	LOCATION: 191 E MAIN ST.	
PROJECT NO.: 12101014	CARLTON, OR. 97111		
DATE: 28 JAN 2021	SCALE: NTS	DRAWN BY: SA	

REGISTERED PROFESSIONAL ENGINEER
OREGON
STEPHEN R. HERTYFORD
JULY 9, 2001
45783
1/28/2021
RENEWS: 6/30/21

South Valley Engineering
4742 Liberty Rd. S #151 • Salem, OR. 97302
Ph. (503) 302-7020 • Fax (888) 535-6341
www.southvalleyengineering.com

Major Site Design Review Application

City of Carlton

SDR 2021-01
 Docket No.: _____
 Date: 2/11/21
 Fee: _____
 Receipt No.: _____

Applicant: Name City of Carlton
 Mailing Address 191 E. Main St.
Carlton, OR 97111
 Phone 503-852-7575

Title Holder: Name City of Carlton
 Mailing Address 191 E. Main St.
Carlton, OR 97111

Location: Street Address 191 E. Main St.
 Tax Lot Number 02800 Map R3421AD

Description: Comprehensive Plan Designation Public Facility (PF)
 Current Zoning DD-RP

Prerequisites: In accordance with Development Code Section 17.156.020, the Planning Commission shall have the power to hear and decide site design review requests. To request a hearing and approval of a site design review request by the Planning Commission, there shall be submitted to the City Recorder in addition to this application and filing fee:

A Site Analysis that describes the following:

1. Existing site topography;
2. Identification of areas exceeding 10% slopes;
3. Site drainage, areas of potential flooding;
4. Areas with significant natural vegetation;
5. Classification of soil types;
6. Existing structures, roadway access and utilities;
7. Fire flow information.
8. Existing and proposed streets, bikeways, and pedestrian facilities within 300 feet.

A Site Plan (8½ x 11 inches or multiples thereof) illustrating the following information:

1. Proposed grading and topographical changes;
2. All proposed structures including finished floor elevations, setbacks, exterior elevations, and exterior finishing.
3. Vehicular and pedestrian circulation patterns, parking, loading and service areas;
4. Proposed access to public roads and highways, railroads or transportation systems;
5. Site drainage plan including methods of storm drainage, sanitary sewer system, water supply system and electrical services. Invert elevations may be required for all underground transmission lines;
6. Proposed landscape plan, to include appropriate visual screening and noise buffering, where necessary, to ensure compatibility with surrounding properties and uses;
7. Proposed on-premise signs, fencing or other fabricated barriers, together with their heights and setbacks;
8. Proof of ownership and signed authorization for the proposed development if applicant is not the owner of the site; and
9. A schedule of expected development.
10. A traffic impact analysis if requested by the City Manager.
11. Other appropriate studies and information that may be required by the City Manager to adequately evaluate the project.

The names and addresses of all property owners within 100 feet of the site boundaries, as shown on the last preceding tax roll of the Yamhill County Assessor. Note: A list of property owner names and addresses within 100 feet of the property may be obtained from a title company or the Yamhill County Assessor Department located at: 535 NE 5th Street, Room 42, McMinnville, OR, phone: (503) 434-7521.

A detailed description of the proposed development.

One (1) paper copy and **one (1) electronic copy** (PDF format preferred) of this application and all of the application attachments. Copies must be clear and legible.

17.156.060 Evaluation of the Site Plan

The review of a Site Plan by the Planning Commission shall be based upon consideration of the following:

- A. Conformance with the General Development Standards Chapters 17.60 through 17.104 of this title;
- B. Application of the standards of this title relative to the characteristics of adjoining and surrounding uses;
- C. Drainage and erosion control requirements relative to applicable city and DEQ standards, and engineering best practices;
- D. Public health factors relative to applicable building codes, sanitation requirements and city standards;
- E. Parking, traffic safety, and connectivity of internal circulation to existing and proposed streets, bikeways and pedestrian facilities;
- F. Provision for adequate noise and/or visual buffering from non-compatible uses or activities;
- G. Balancing retention of natural features with individual property rights and growth management policies; and
- H. Avoidance of natural hazards.

Conditions of Approval:

As specified by Development Code Section 17.188.020 (I), approval of a Site Design Review application may be granted subject to conditions. The following limitations shall be applicable to conditional approvals:

1. Conditions shall be designed to protect public health, safety and general welfare from potential adverse impacts caused by a proposed land use described in an application. Conditions shall be related to the following:
 - a. Protection of the public from the potentially deleterious effects of the proposed use; or
 - b. Fulfillment of the need for public service demands created by the proposed use.
2. Changes or alterations of conditions shall be processed as a new administrative action.
3. All conditions of approval required by the city for a Type II or Type III approval shall be completed prior to the issuance of building permits, except that the Planning Commission may stipulate that some conditions be completed prior to issuance of building occupancy permits when the former is impractical. When an applicant provides information demonstrating to the satisfaction of the Planning Commission that it is not practical to fulfill all conditions prior to issuance of a building permit, the applicant must complete a performance guarantee for required improvements pursuant to Chapter 17.216.

Public Improvements

As part of a design review approval, the decision body may impose the following conditions on a new or expanding development to ensure compliance with the city's public facility standards:

- A. Limit or prohibit access to streets, including requiring consolidation or reconfiguration of existing accesses;
- B. Require transportation improvements, including but not limited to new or widened streets, sidewalks, bicycle lanes, on-street parking, roadway markings, traffic controls, but transit waiting areas, or other improvements consistent with the city's transportation system plan or as recommended in a traffic impact analysis pursuant to Chapter 17.100;
- C. Require the dedication of additional right-of-way and/or street improvements where necessary to meet city street standards; and
- D. Require the replacement, extension, and/or upgrade of other essential infrastructure, public or private, including but not limited to water, sewer and storm drainage facilities, provided the required improvements must be necessary to serve the proposed development in conformance with city standards, and the cost borne by the developer/applicant must be roughly proportional to the impact the development is expected to have on those facilities.

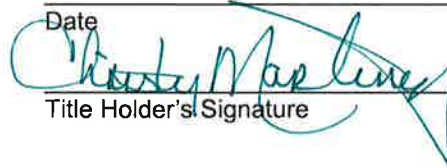
I HEREBY CERTIFY THAT ALL STATEMENTS CONTAINED HEREIN, ALONG WITH THE EVIDENCE SUBMITTED, ARE IN ALL RESPECTS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.


Applicant's Signature

Date

Applicant's Signature

Date


Title Holder's Signature

Date

Title Holder's Signature

Date

NOTE: ALL OWNERS MUST SIGN THIS APPLICATION OR SUBMIT LETTERS OF CONSENT. INCOMPLETE OR MISSING INFORMATION MAY DELAY THE APPROVAL PROCESS.

CITY OF CARLTON – DEVELOPMENT

APPLICATION COMMENTS

City Hall Parking Lot & Police Garage - Application

Note that tax lot 2800 includes City Hall, but the proposed improvements are on the northern half of the lot. The proposed building replaces an existing building, and the parking lot is covering an area that is existing parking lot and a former building.

1. **N. First St.: code Section 17.60.030** - This is an existing collector street per the TSP. The requirements for existing collector streets are: 55' ROW, 40' pavement, curb & gutter, and a 6' sidewalk. The existing conditions are a 40' ROW, 18' pavement, no curb & gutter and no sidewalk.
 - a. Street improvements are required to be constructed along the entire frontage of the property, which would be on First St. all the way to E. Main St., and along E. Main St.
 - i. However, the E. Main St. frontage is already complete and there is a City project to improve the condition of the improvements on E. Main St.
 - ii. First St. on the southern part of the property is adjacent to the City Hall. The City is contemplating improvements/expansion to City Hall. It is reasonable to have the First St. street improvements be constructed with the City Hall project to allow the design to be coordinated.
 - iii. Based upon the above discussion it is recommended that the street improvements only be required along the frontage of the parking lot.
 - b. To meet code there would need to be a 7.5' dedication of ROW, an additional 11' of pavement on the west side, curb & gutter and a 6' sidewalk. However, there are conditions that will limit this.
 - c. While the street is designated as a collector street it acts more like a local street, with minimal traffic.
 - d. Existing conditions north of Monroe St. the east side of the street is set with curb & gutter, and the pavement is approximately 25' wide. This portion of the street will never be made wider as the property is already developed.
 - e. Along First street in the project location there are four existing buildings that will limit the dedication of ROW which includes City Hall, two residential structures on the east side of First St., and a residential structure on the west side of First St.
 - f. Based upon the limitations it is recommended that the ROW remain as it is, and the street improvements match the built out conditions to the north. This would include a 5' sidewalk, curb & gutter and 25' of pavement. This would allow two-way traffic but no on-street parking.

2. **Access: Code section 17.100.040 F** - The proposed parking lot shows two driveways accessing First St. This allows for efficient circulation of traffic through the parking lot with one-way movement, and will facilitate access for the police vehicles to the garage structure. The proposed layout is reasonable and acceptable. Signage will be required at the entrance and exit for traffic control.

3. **Water: Code Section 17.76.020 & 17.88.050** - There is a 2" water line on N. First St. that extends from E. Monroe St. to approximately the mid-point of the proposed parking lot. There is a fire hydrant at the intersection of N. First St. and E. Main St. that has sufficient fire flow for the City Hall.
 - a. While the water line does not extend across the frontage of the property, there is already water service to the tax lot in question and every tax lot along the frontage of the property. There is no need to extend the water line to provide service or facilitate further development to adjacent properties.
 - b. Fire flow is provided to the property from the fire hydrant on E. Main St. There may be distance issues to the back of the property.
 - c. A 2" water line is undersized and should be upgraded at some point. The City has a CIP project projected to be within the next five years that would extend a new water line on First St. from E. Main St. to E. Monroe St. which would include an additional fire hydrant at the intersection of First St. and E. Market St.
 - d. As the existing water service is adequate, and the City has a CIP project to address upsizing the water line in the near future, it is recommended that no public water line improvements would be required at this time.
4. **Water Service:** Each lot is required to have an individual water service. The property already has water service, so no improvement would be required.
5. **Sanitary Sewer: Code Section 17.76.020 & 17.88.050** - There is an 8" sanitary sewer pipe along the entire property frontage on E. Main St. that provides sanitary sewer service to the property. There is no sanitary sewer on First St. All properties along First St. between E. Main St. and E. Monroe St. already have sanitary sewer service, so there is no need to extend the main line in this location.
6. **Sanitary Service:** Each lot would be required to have an individual sanitary service. There is existing service to the property, so no improvement is required.
7. **Storm Drainage: Code Section 17.72 & 17.88.050** - Currently, the storm drainage from the parking lot is collected through area drains connected to an 8" concrete pipe that discharges to an existing storm ditch at the intersection of First St. and E. Market St. The ditch enters a culvert under a driveway. The pipe connects to a pipe on Second St. which extends to E. Main St. and eventually discharges to Hawn Creek.
 - a. The proposed storm drainage utilizes the same system using two area drains in new locations in the parking lot, and a catchbasin connected to the existing 8" pipe. As this is draining the same area which is currently either pavement, building or crushed rock it is reasonable to expect that there would be marginal or no additional flow.
 - b. During the design phase a storm drainage report should be conducted to verify the pre and post development storm flow, and the capacity of the ditch. If the ditch has insufficient capacity, then additional storm facilities may be required. This could come in the form of ditch capacity improvements, low impact development storm system improvements, or detention.

